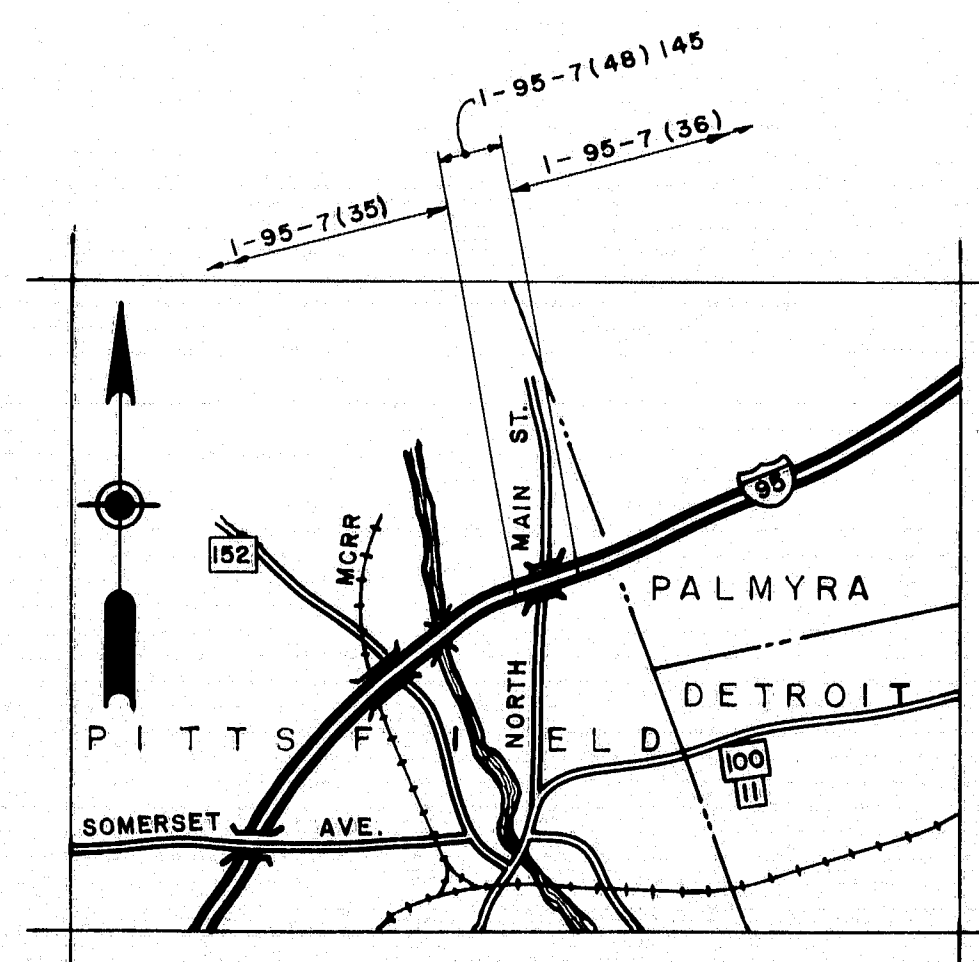


STATE OF MAINE
STATE HIGHWAY COMMISSION



INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY

FEDERAL AID PROJECT NO. I-95-7(48)145
LENGTH OF PROJECT 0.052 MILES



LOCATION MAP
APPROX. SCALE - 1" = 1 MILE

INDEX OF SHEETS

1	TITLE SHEET
2	GENERAL PLAN (ESTIMATE OF QUANTITIES)
3-4	FOUNDATION SURVEY
5	PROFILES - ROADWAY WORK
6	SOUTHBOUND CROSS SECTIONS
7	NORTHBOUND CROSS SECTIONS
8-9	CROSS SECTIONS - NORTH MAIN STREET
10	ABUTMENT NO. 1 - N.B.
11	ABUTMENT NO. 2 - N.B.
12	ABUTMENT NO. 1 - S.B.
13	ABUTMENT NO. 2 - S.B.
14	PIERS - NORTHBOUND & SOUTHBOUND
15	STRUCTURAL STEEL
16	BLOCKING SCHEDULE (ESTIMATE OF BRIDGE QUANTITIES)
17	SUPERSTRUCTURE
18	SLOPE PAVING
19	REINFORCING STEEL, GRANITE BRIDGE CURB, APPROACH SLAB DETAIL STANDARD DETAILS, BD 101-62, BD 102-62, BD 103-62

TRAFFIC

NORTH MAIN STREET	INTERSTATE 95
510	A.D.T. 1962 5830
715	A.D.T. 1982 7920
86	D.H.V. 950
11 %	T 11 %
60 %	D 60 %
45 MPH	V 60 MPH

APPROVED
MAINE STATE HIGHWAY COMMISSION

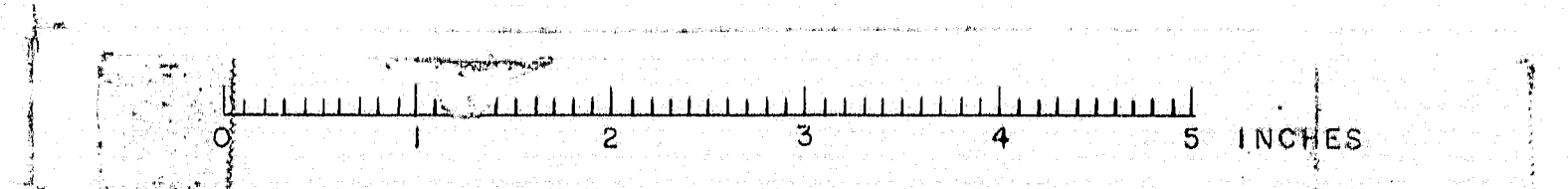
David W. Thomas
CHAIRMAN
E. J. ...
...
CHIEF ENGINEER

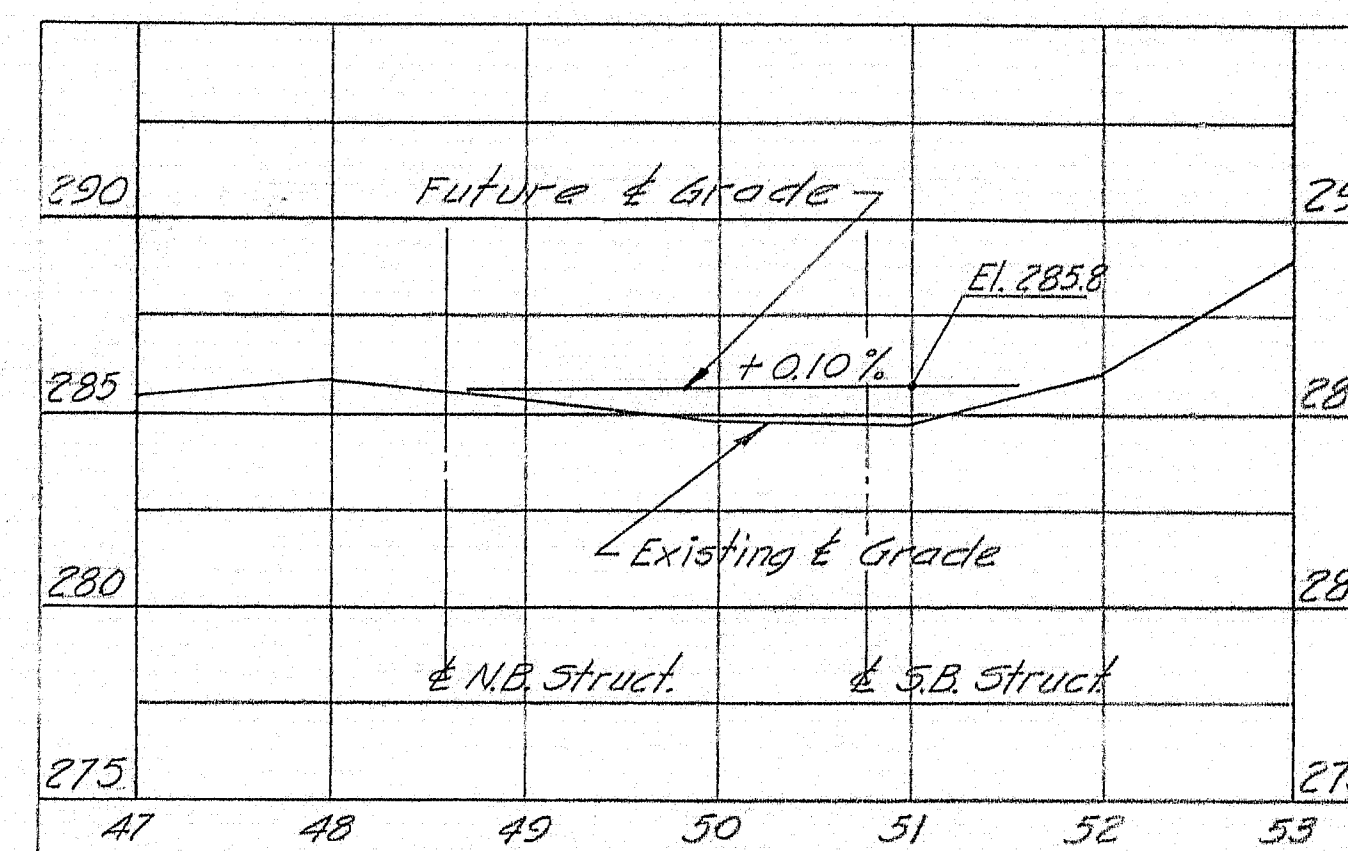
4-10-63
DATE

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
REGION 1

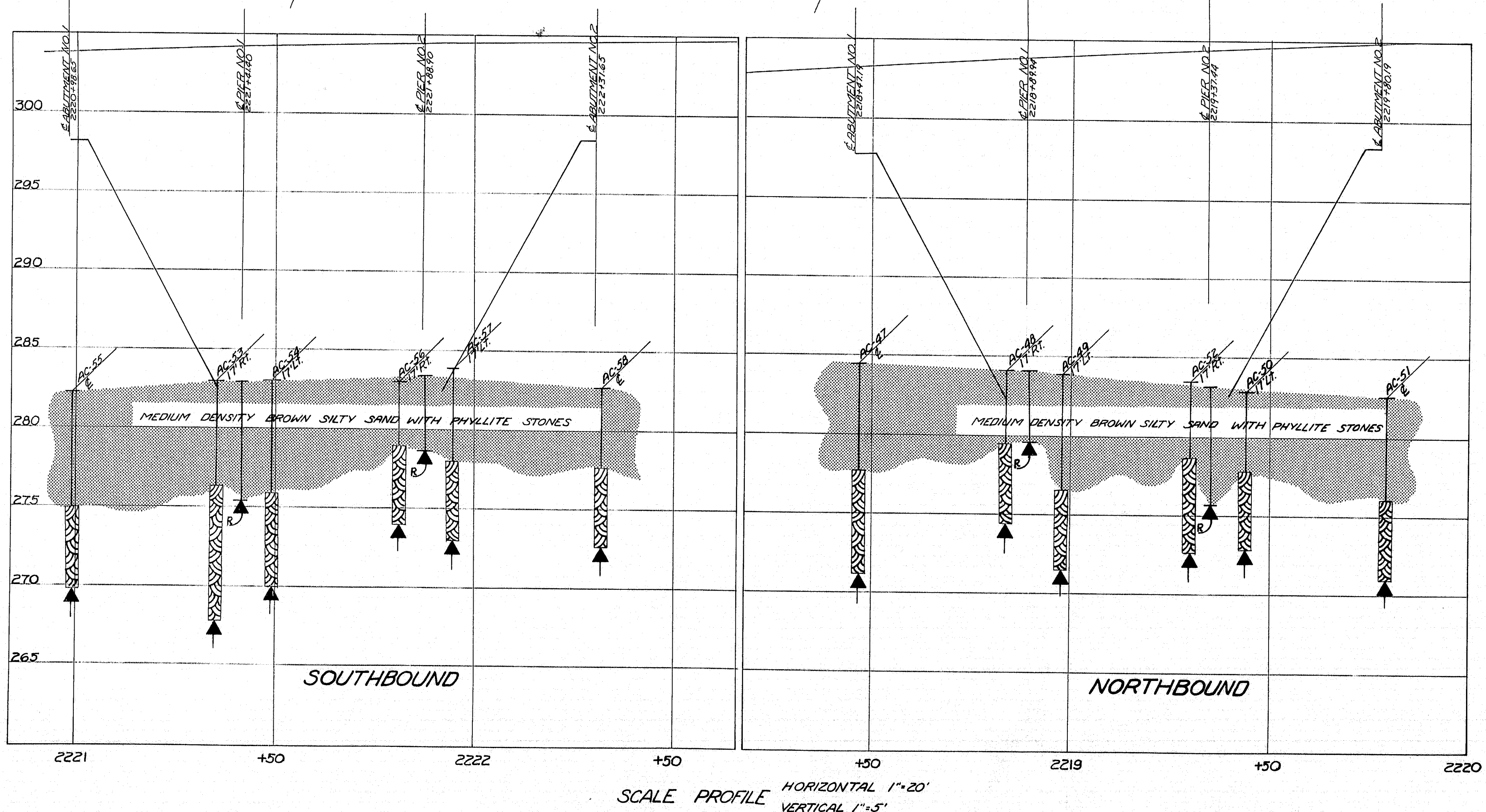
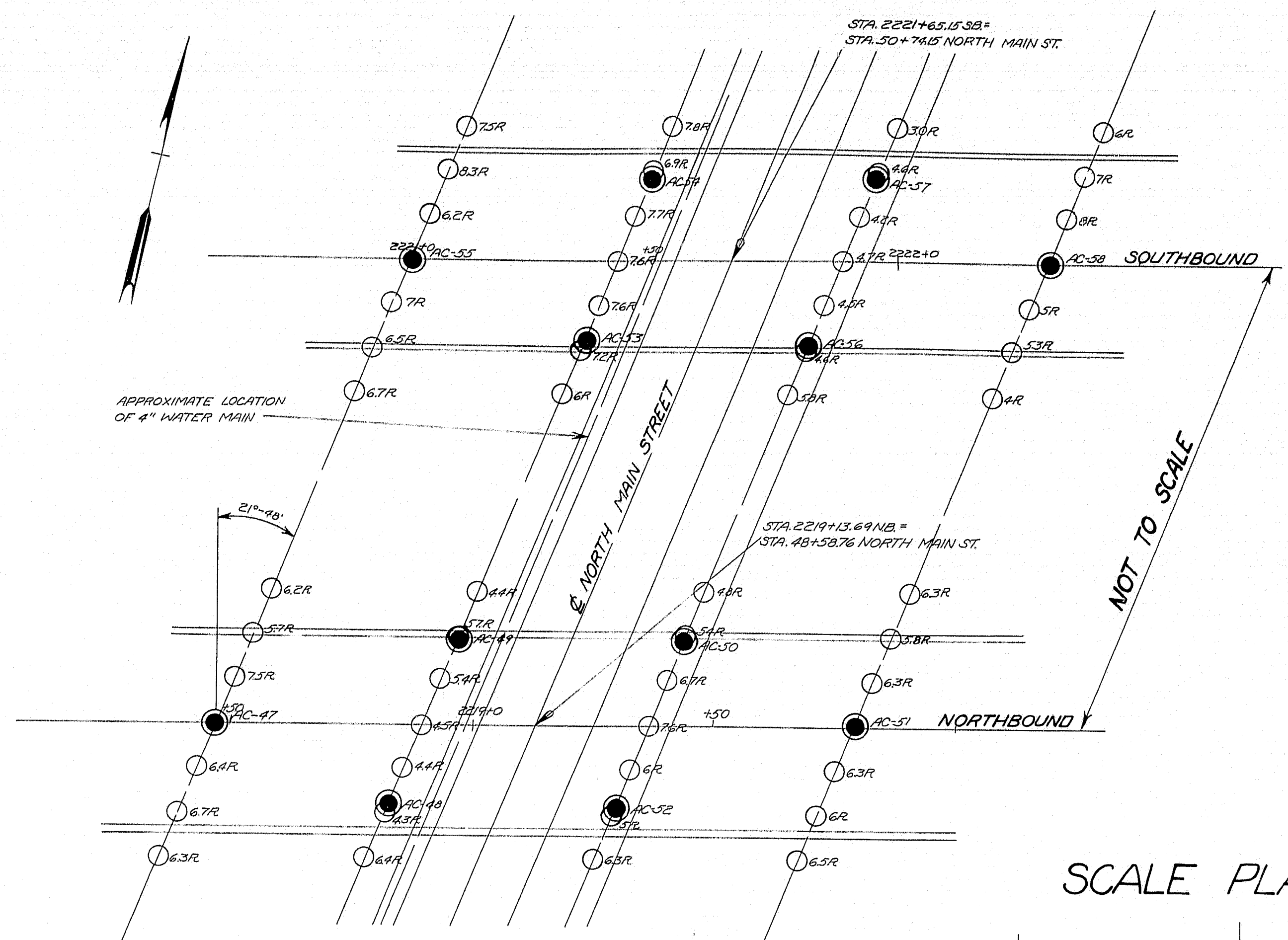
APPROVED

DIVISION ENGINEER DATE

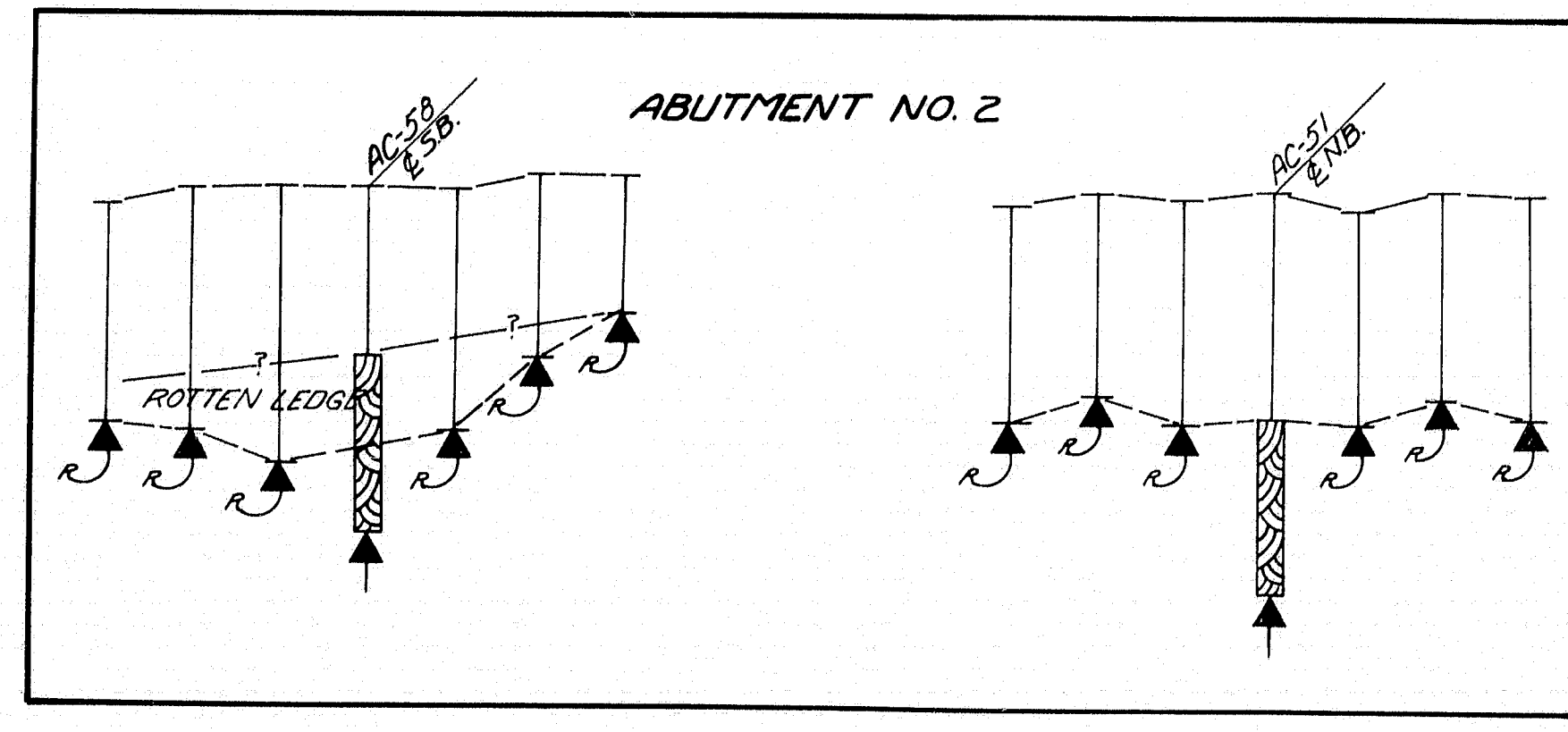
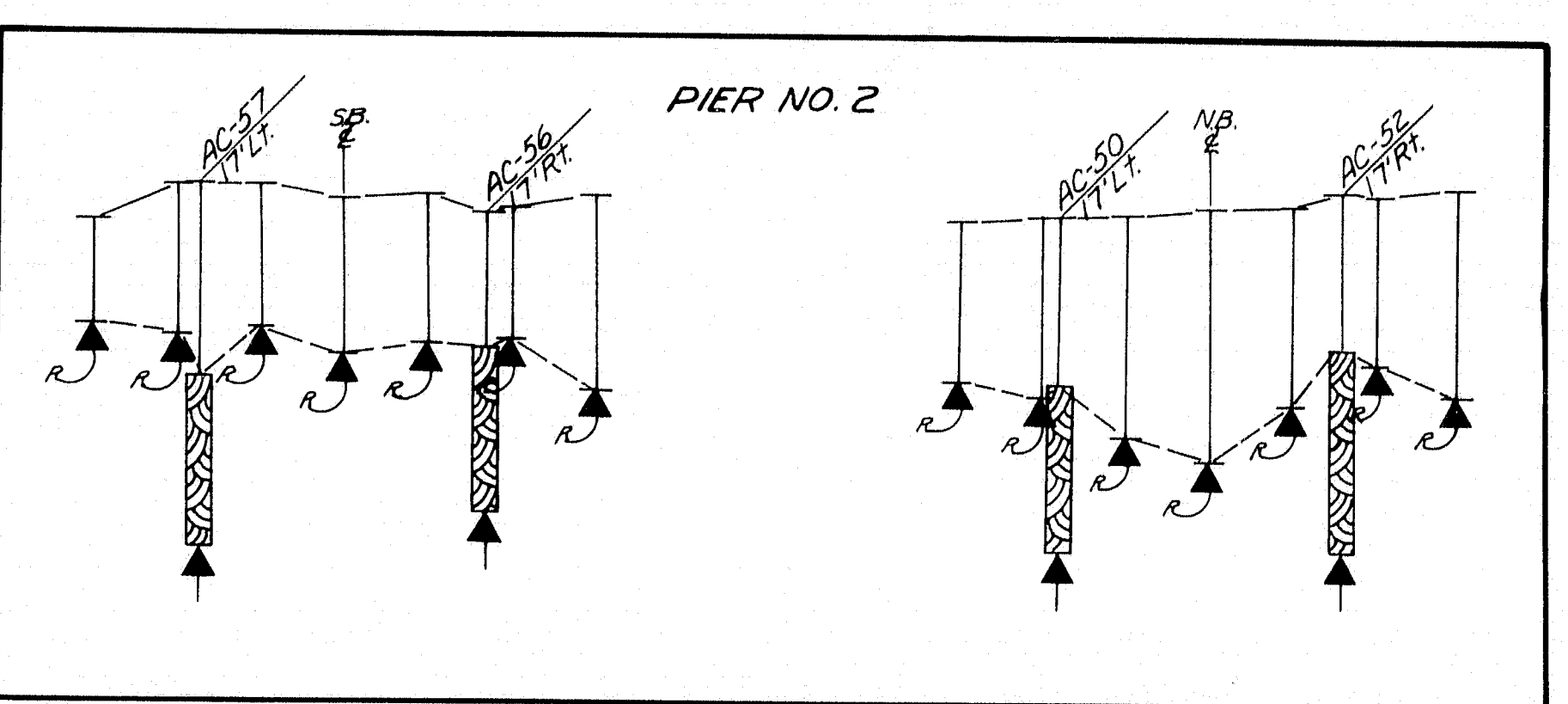
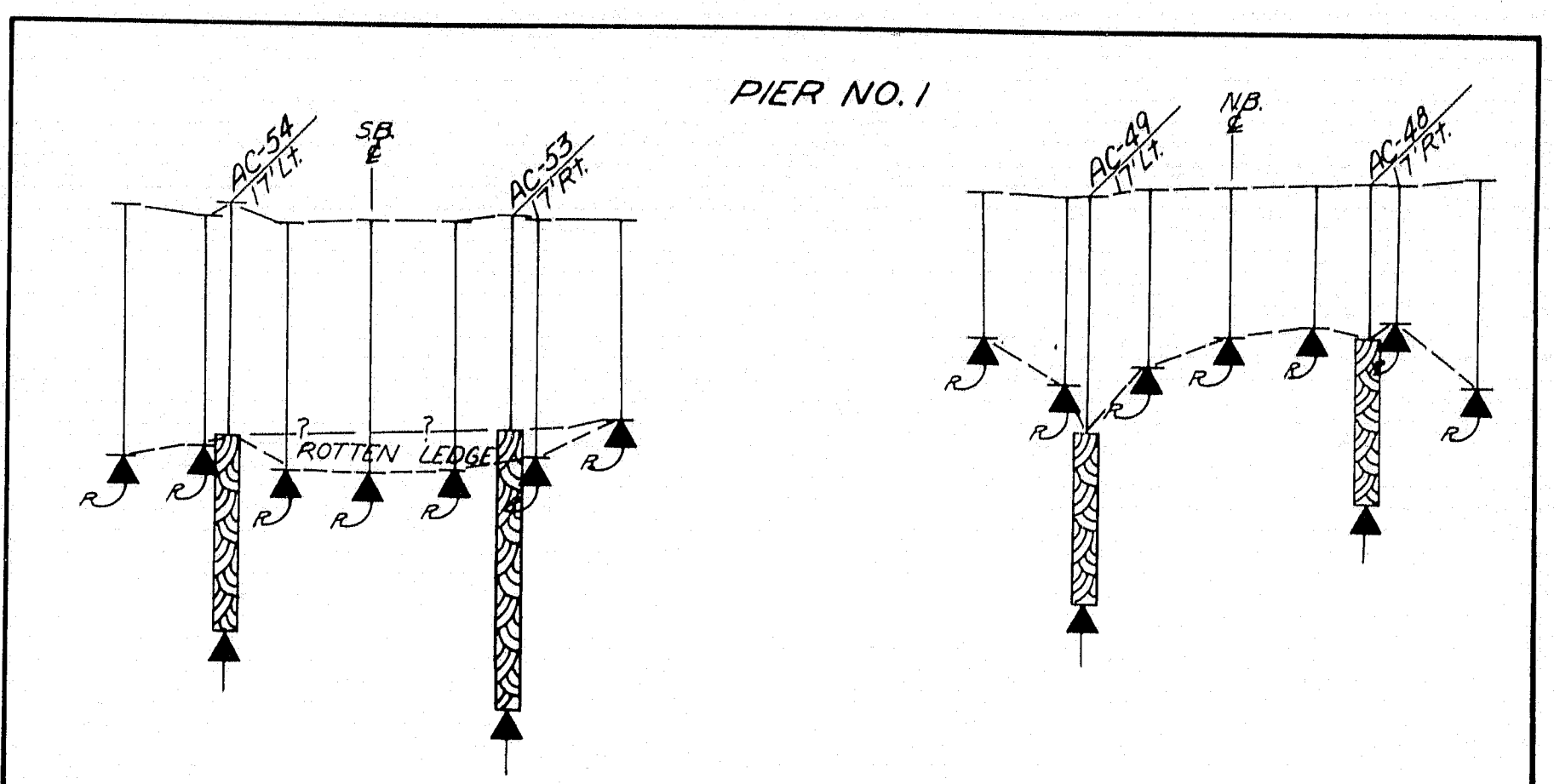
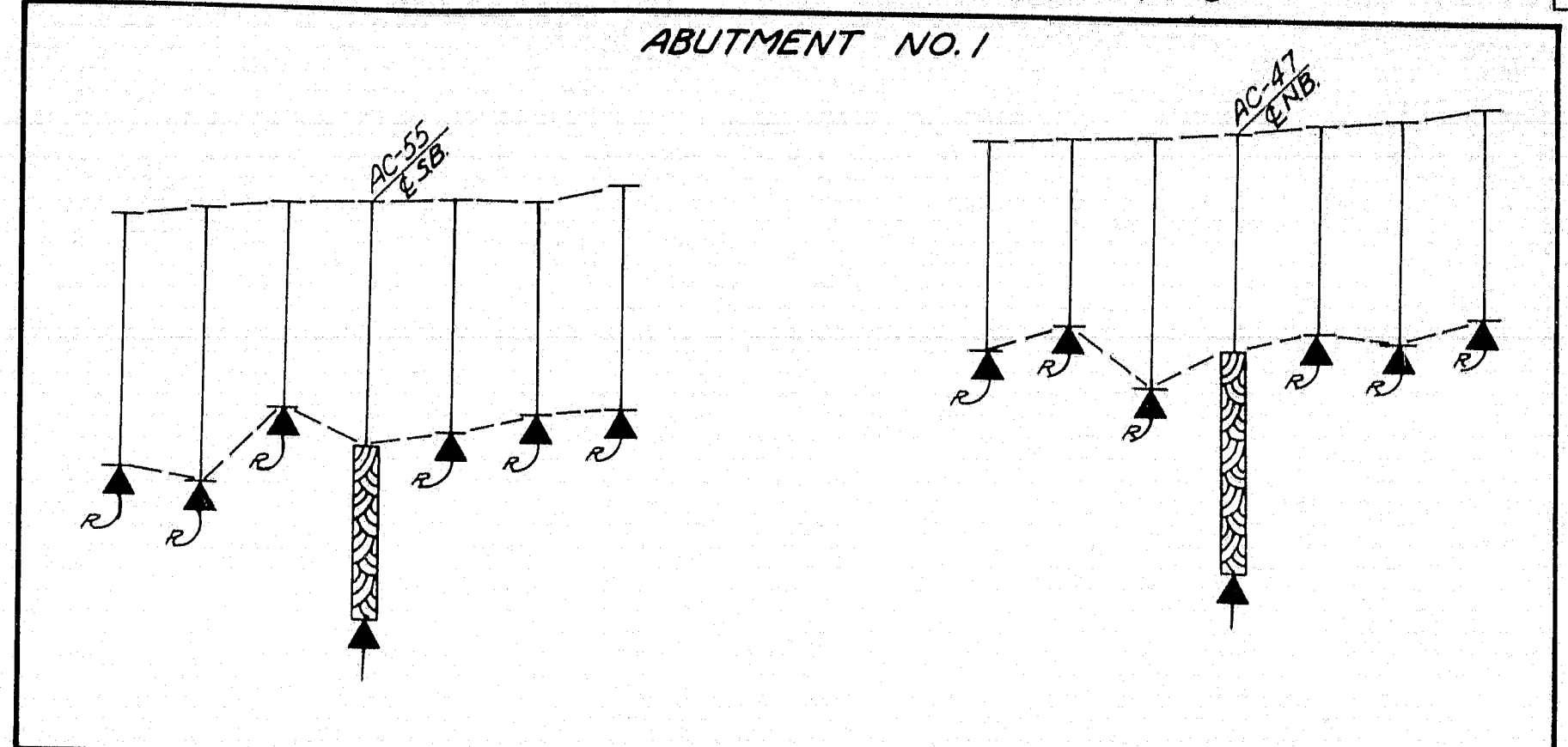




D. R. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-7(18)	3	19



TRANSVERSE SECTIONS



SCALE VERTICAL 1"=5'
HORIZONTAL 1"=20'

DESIGN - J
TRACE - J
CHECK - J

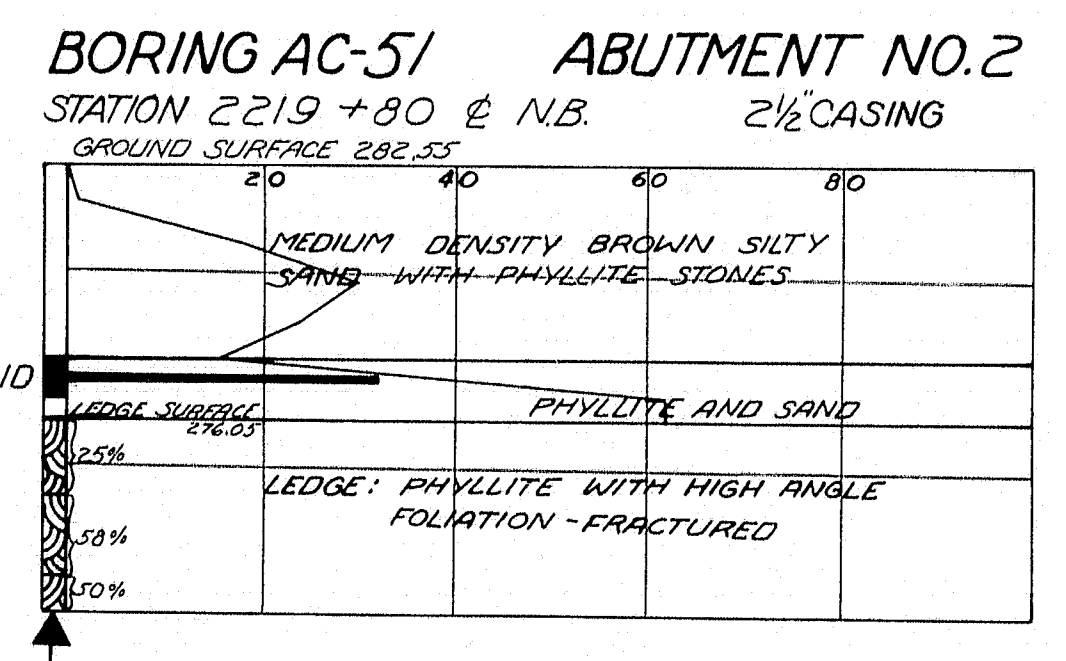
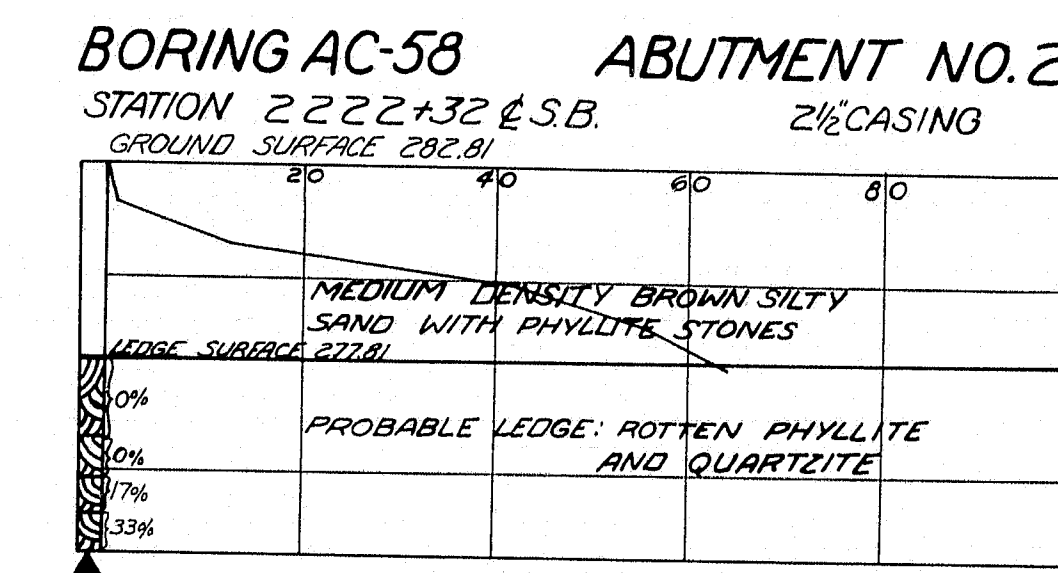
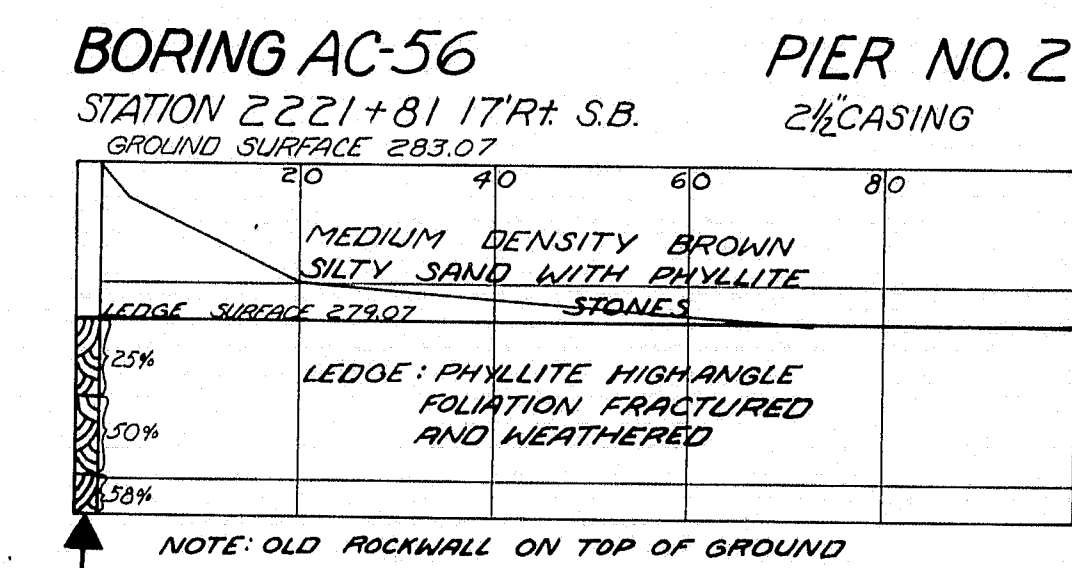
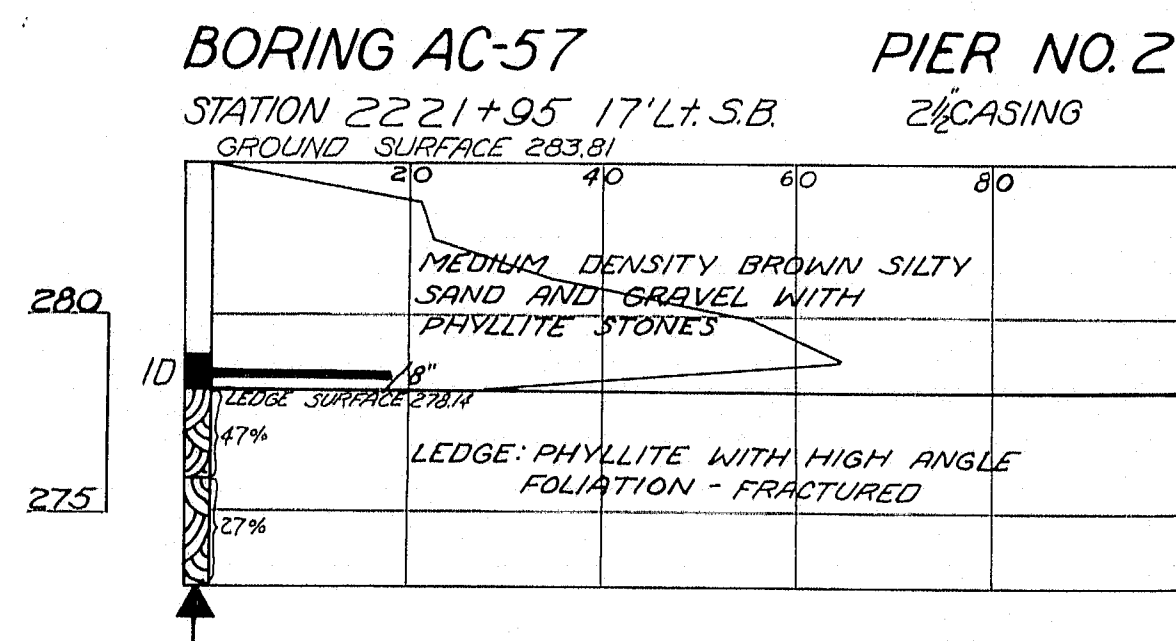
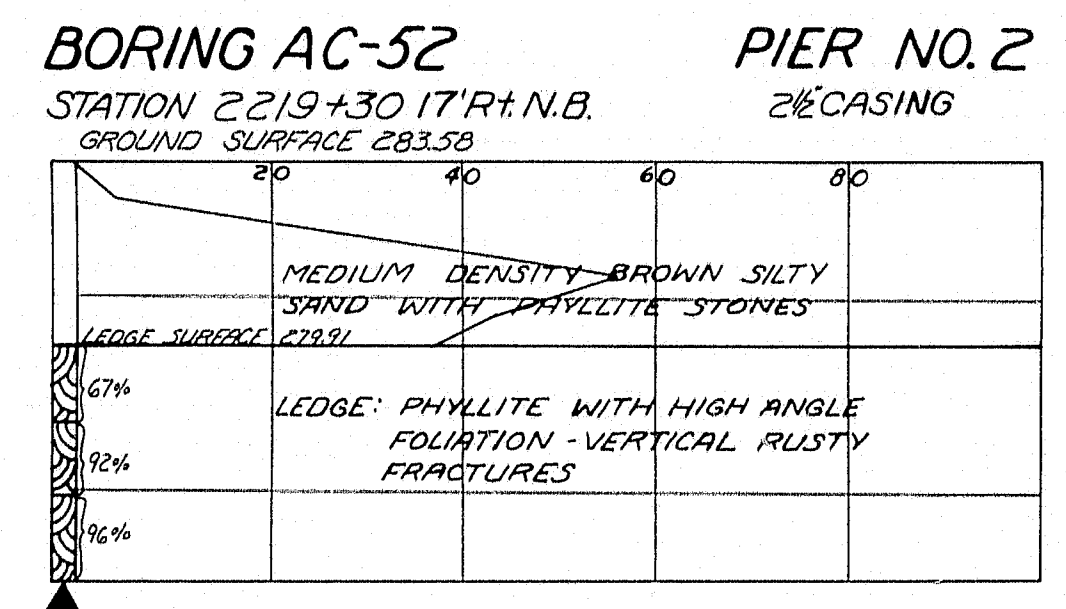
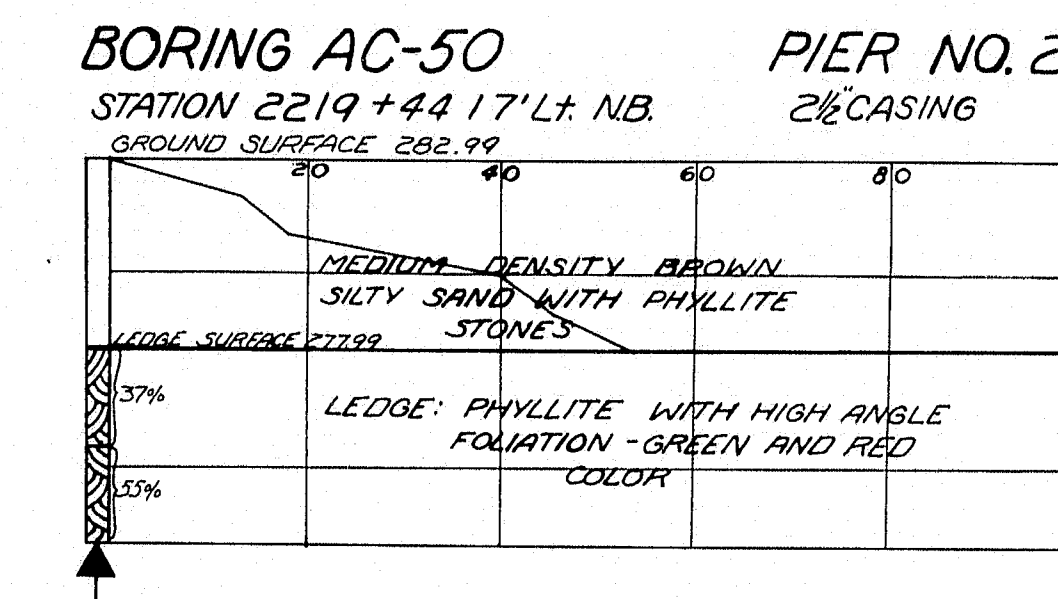
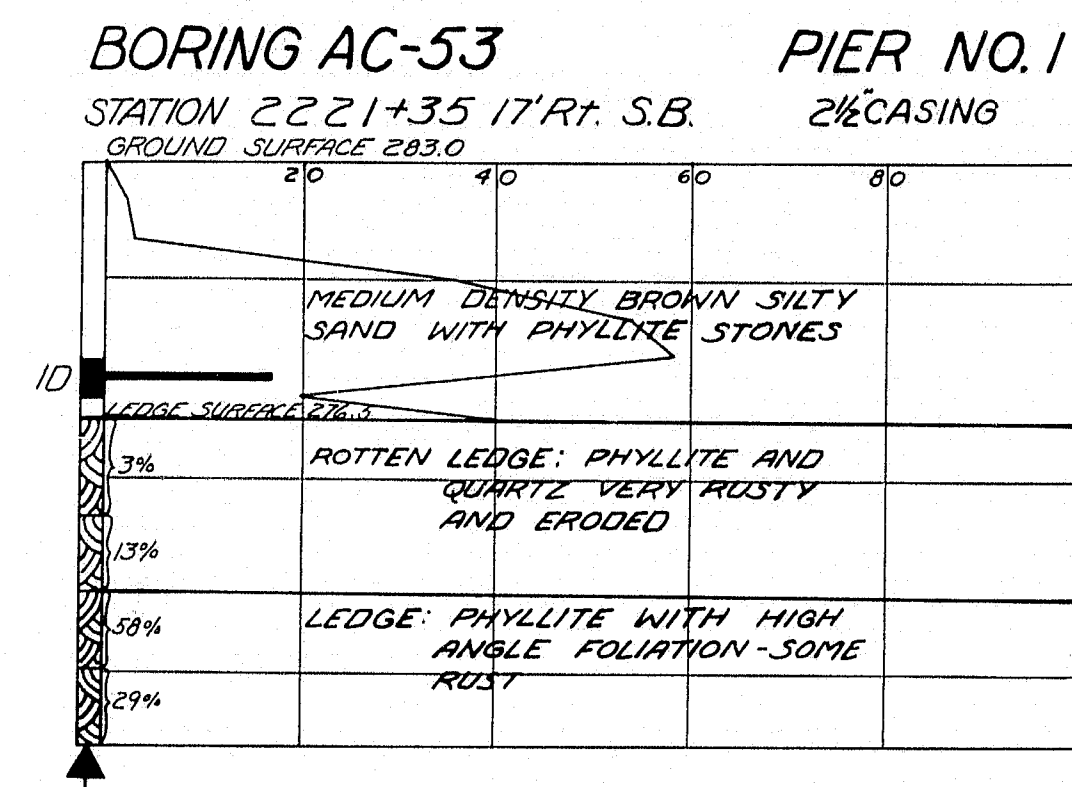
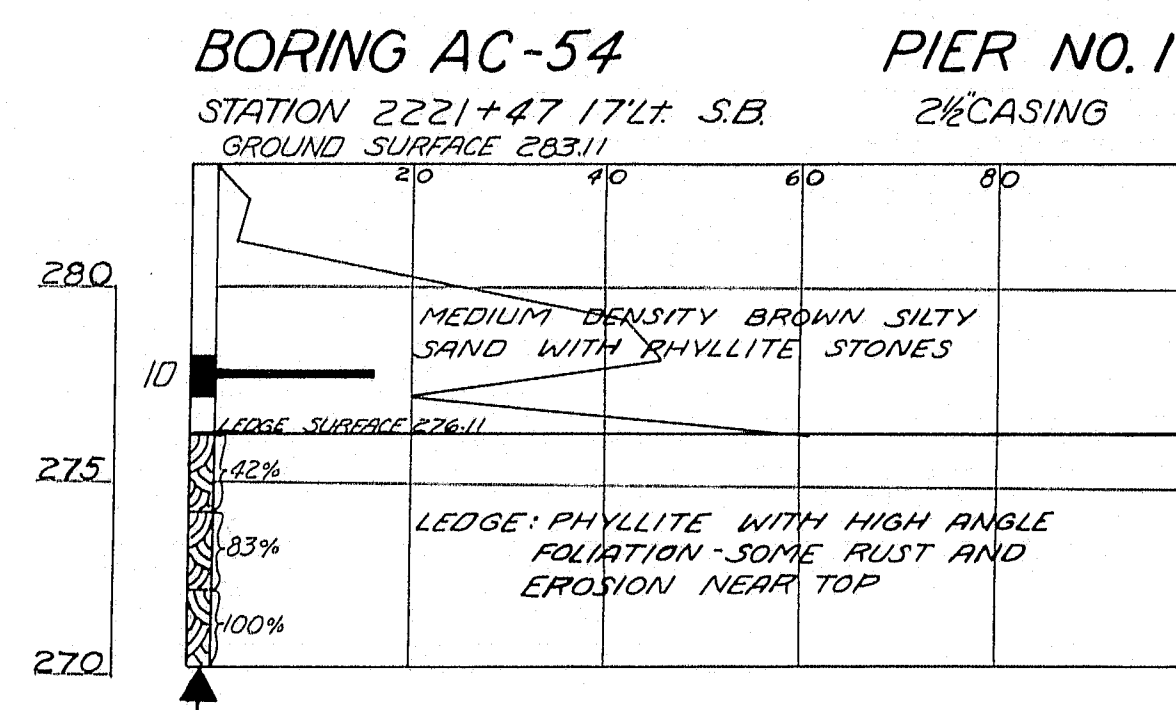
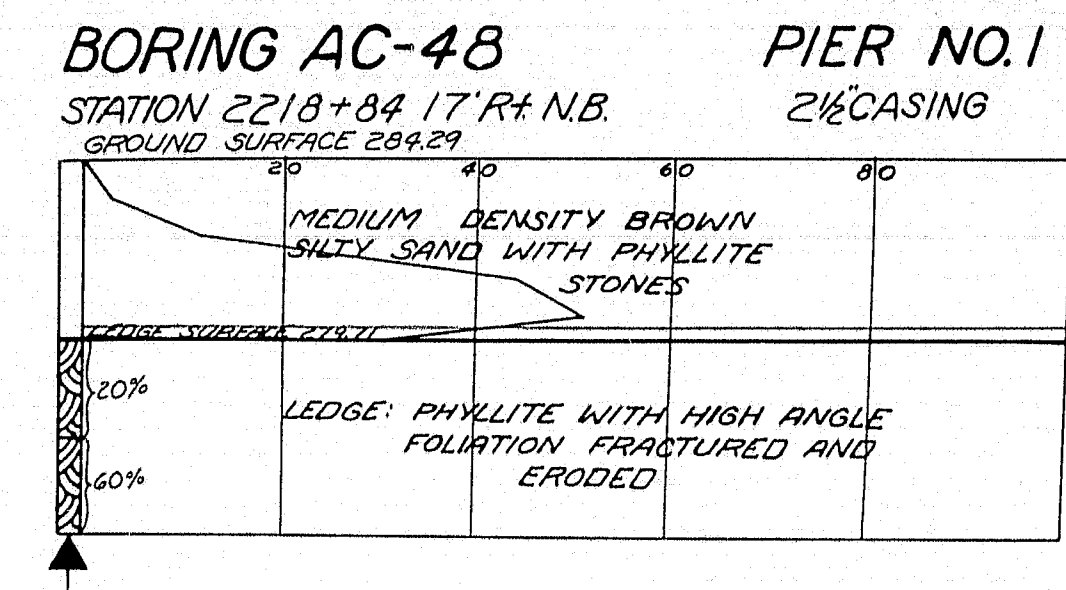
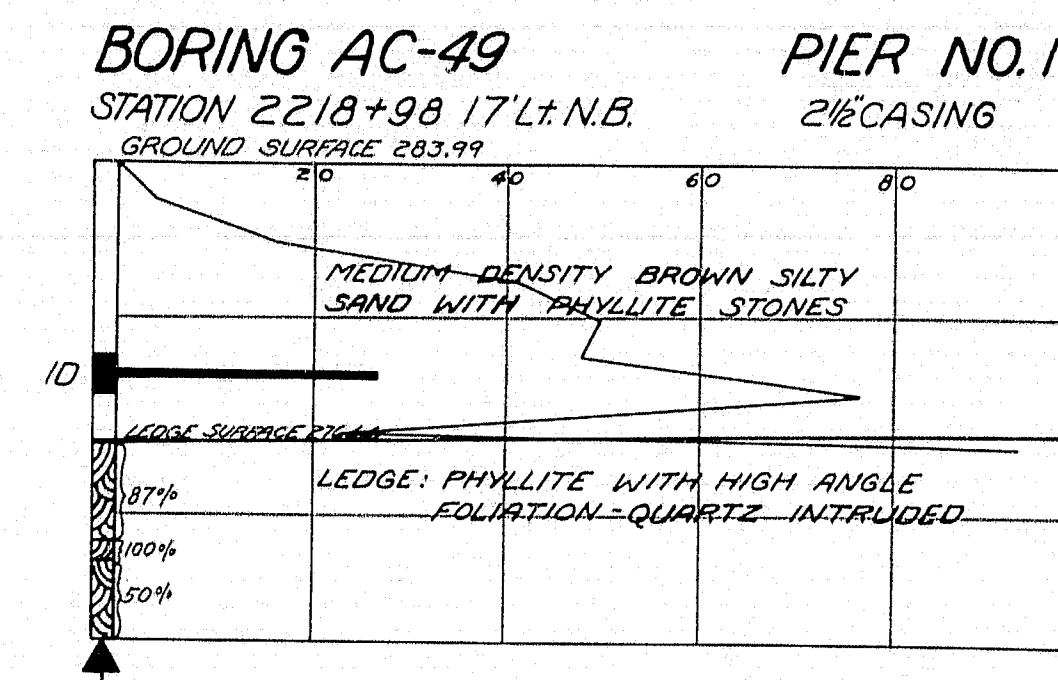
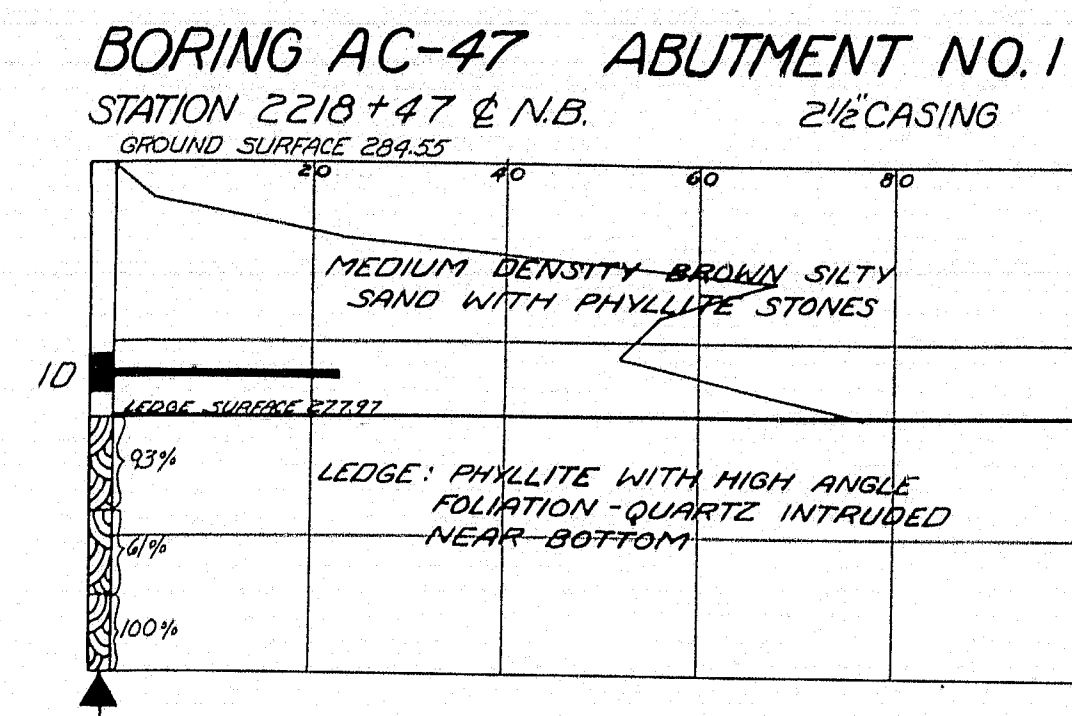
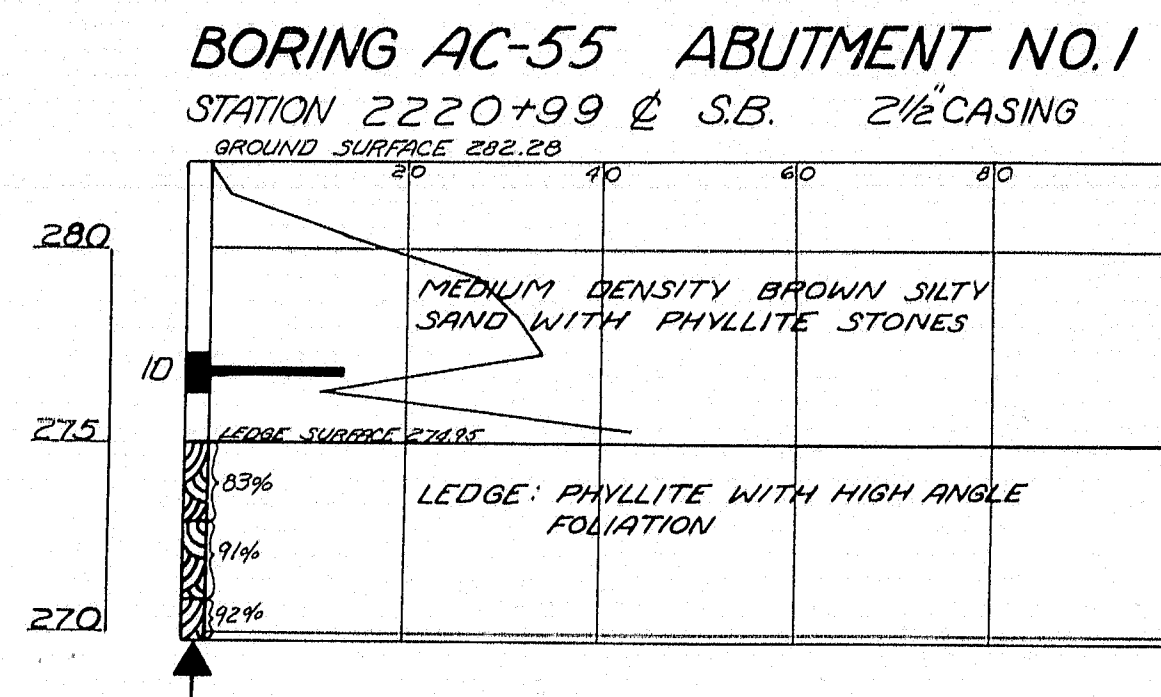
SOILS LAB.

BRIDGE NO.
SURVEY -
PLOT -

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
FOUNDATION SURVEY

SHEET 3 OF 19 AUGUSTA, MAINE, MARCH 1963

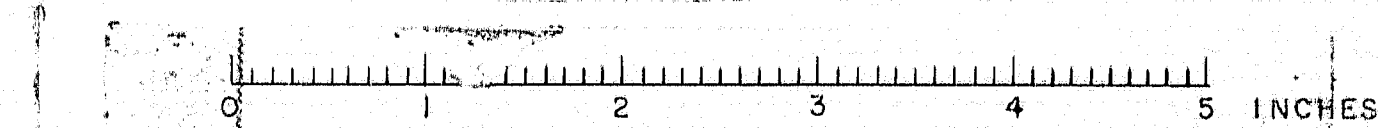


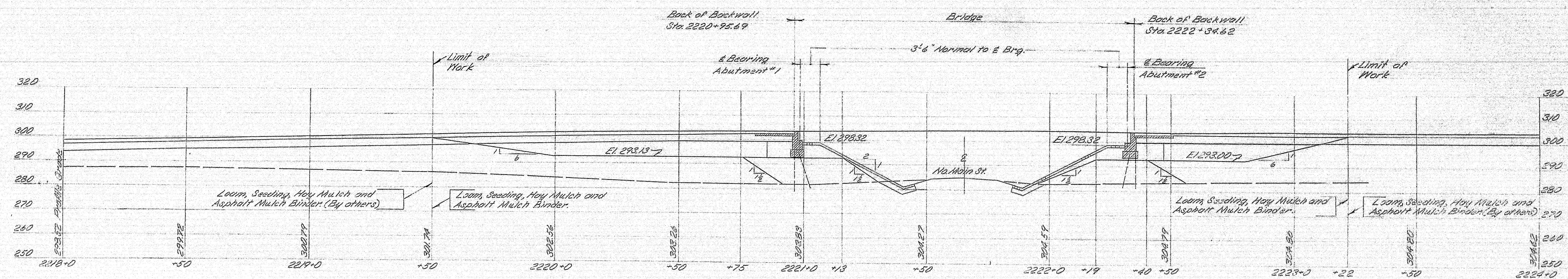
BORING NOTES

- ALL SAMPLES AND VANES ARE MADE AHEAD OF CASING
- NUMBER OF BLOWS REQUIRED TO DRIVE EXTRA HEAVY CASING ONE FOOT WITH 400 FT. LBS. OF ENERGY PER BLOW
- LOCATION OF SAMPLE OR SAMPLE ATTEMPT
- NUMBER AND TYPE OF DRY SAMPLE S&H SAMPLER #1290'S
- NUMBER OF BLOWS REQUIRED TO DRIVE SPOON OR TUBING ONE FOOT WITH 350 FT. LBS. OF ENERGY PER BLOW
- BOTTOM OF BORING (MAY NOT BE BOTTOM OF SOIL STRATA)
- LOCATIONS CORED BY DIAMOND BIT AND PER CENT RECOVERY OF ROCK

DRIVING RESISTANCE
BLOWS / FOOT

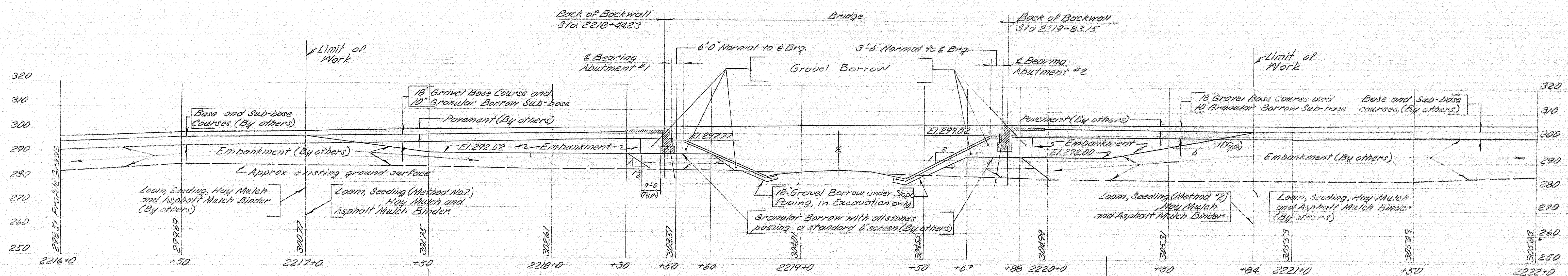
DESIGN - 7	BRIDGE NO.
TRACE - 7	SURVEY -
CHECK - 7	PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER NORTH MAIN STREET IN THE TOWN OF PITTSFIELD SOMERSET COUNTY FOUNDATION - SURVEY	
SHEET 4 OF 19 AUGUSTA, MAINE MARCH 1963	





SECTION ALONG E INTERSTATE SB

All notes on NB section apply to this section except as noted.



SECTION ALONG E INTERSTATE NB

NOTES

See General Plan for ultimate location of berm lines, guard rails and pavement.

Before piers are driven, Granular Borrow and Gravel Borrow shall be placed and compacted to elevation of bottom of footing or abutments.

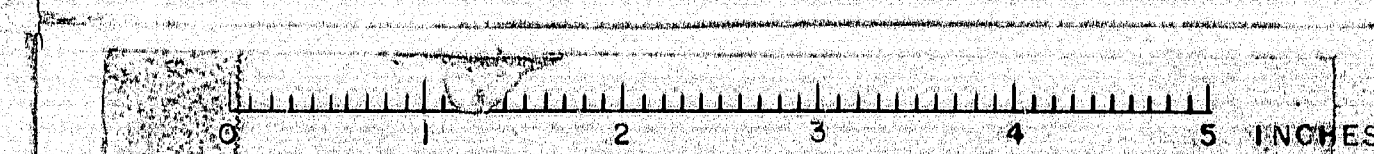
The controlled density method shall be used for placing embankment, granular, and Gravel Borrow under this contract.

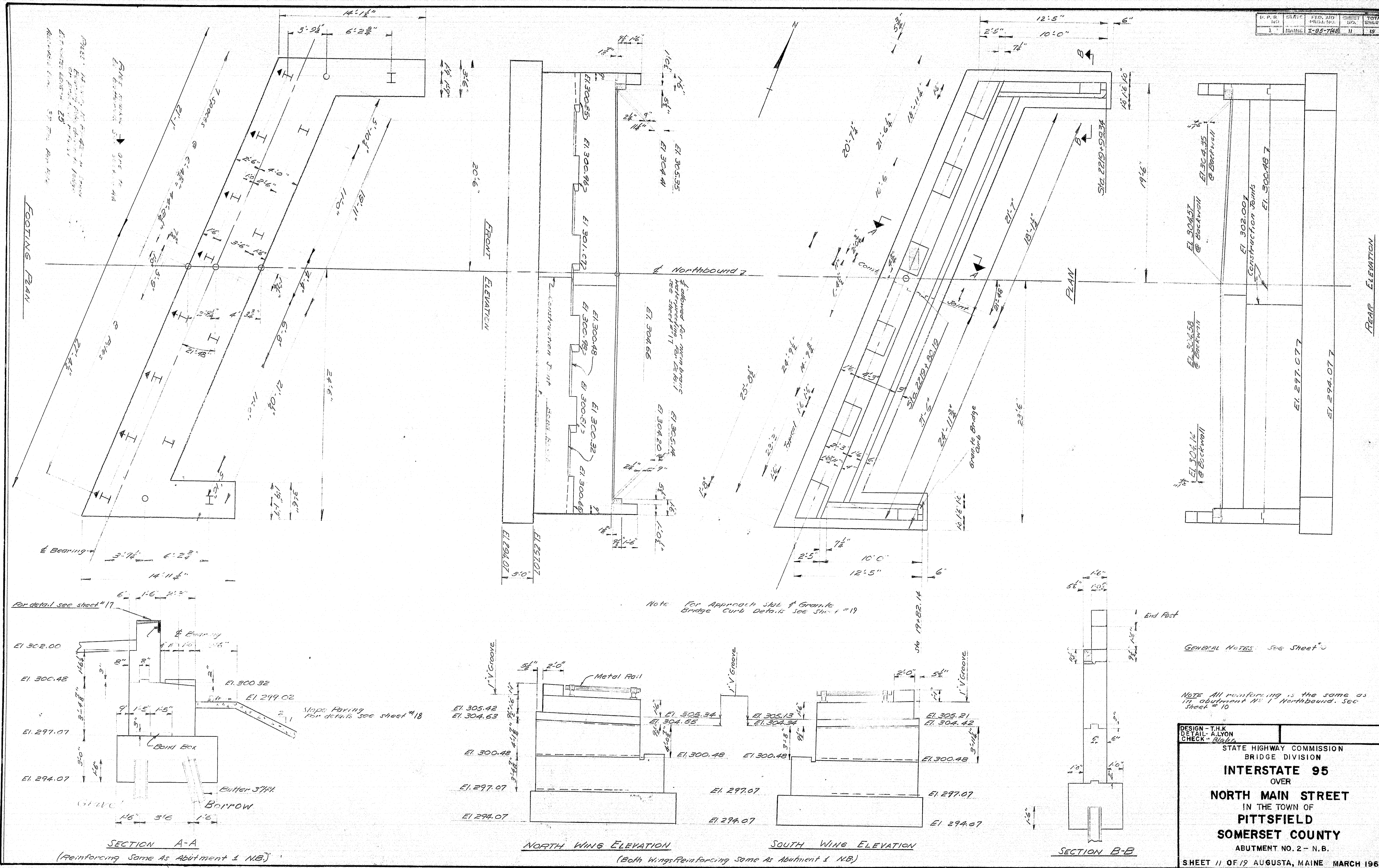
The 18" gravel Borrow under Slope Paving may be reduced or omitted, if in the opinion of the Engineer, the existing material is suitable.

Payments for excavation for Gravel Borrow under Slope Paving to be made under Item 204-18, Structural Earth Excavation, Piers.

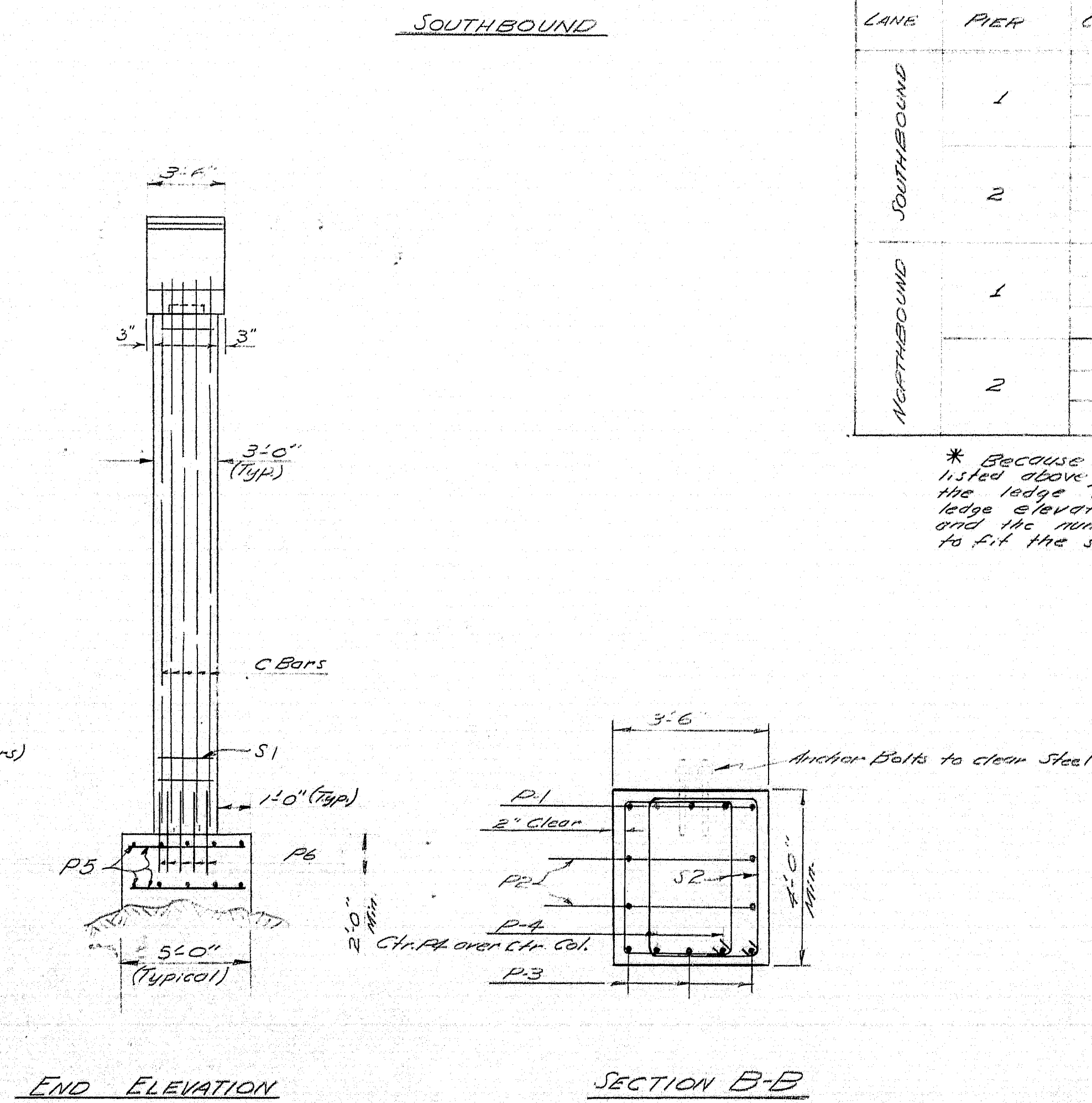
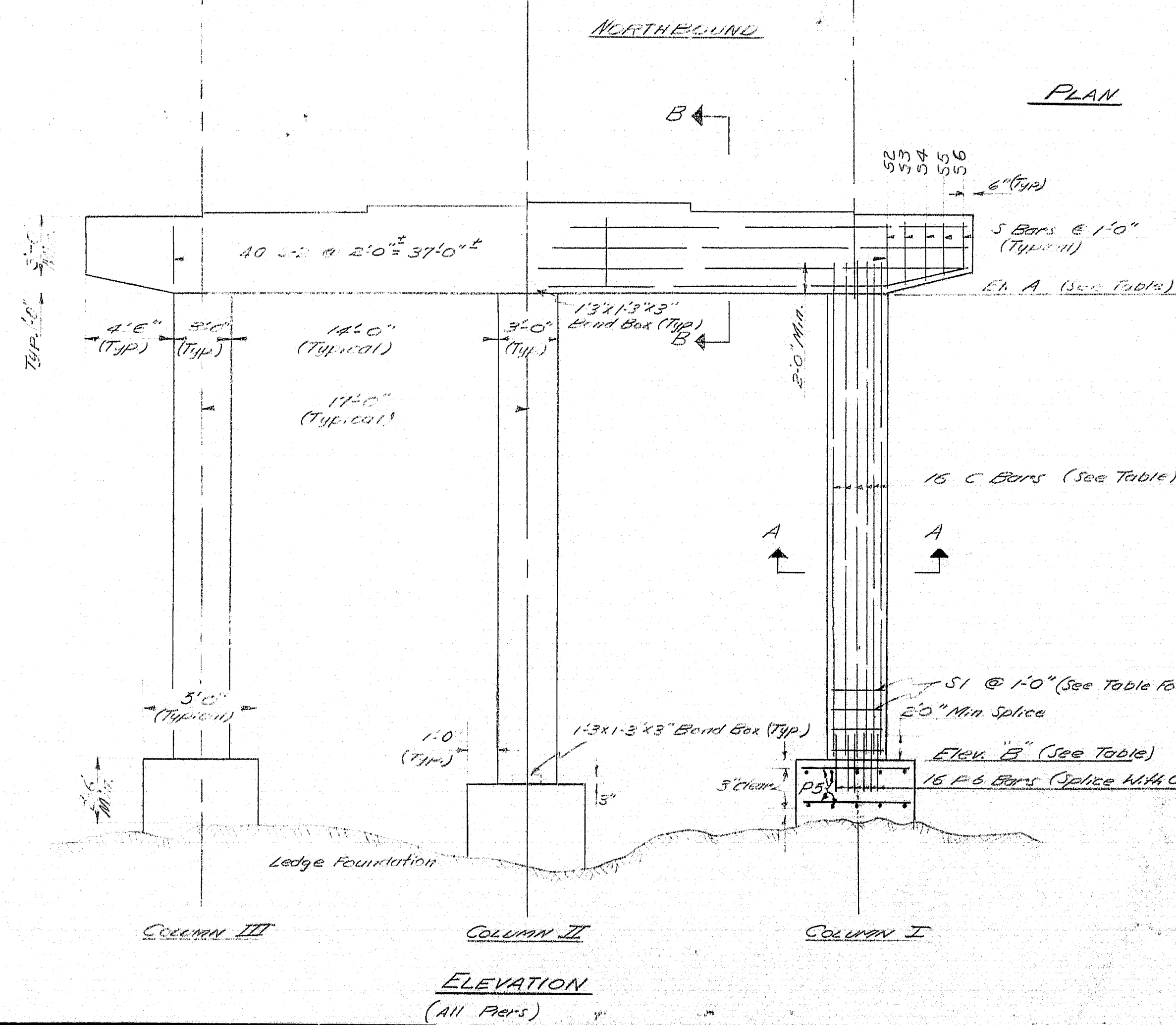
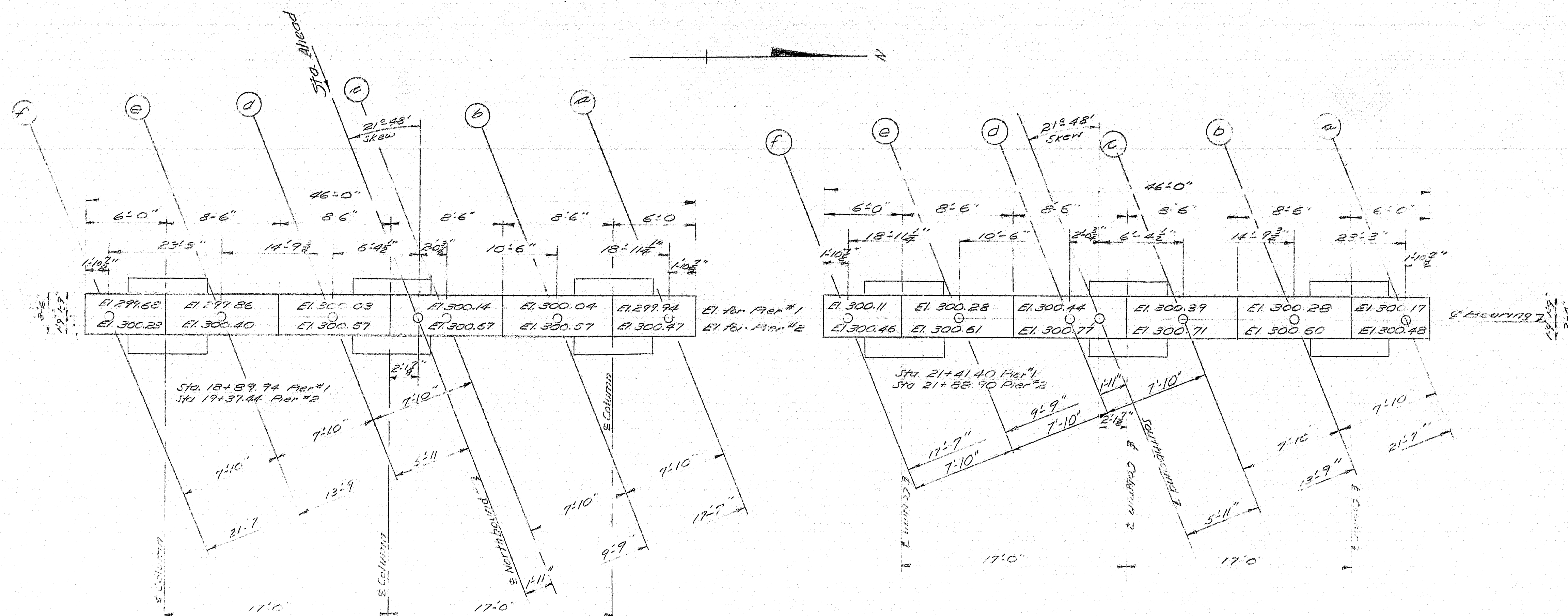
Guard rails to line up with inside face of concrete and posts on bridge.

DESIGN - T. H. K.	BRIDGE NO.
TRACE - CHACE	SURVEY -
CHECK - B. G. K.	PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95	
OVER	
NORTH MAIN STREET	
IN THE TOWN OF	
PITTSFIELD	
SOMERSET COUNTY	
PROFILES - ROADWAY WORK	
SHEET 5 OF 19 AUGUSTA, MAINE MARCH 1963	





GENERAL NOTES:
 All weathered and/or broken ledge to be removed before any footing concrete is placed.
 Dress bearing areas 1' larger, all around, than masonry piers, and to exact elevations shown. Caulk around edges of masonry piers with an approved caulking material. Payment to be incidental to contract items.
 If sound ledge is encountered and excessive rock excavation would be required to obtain a 2'-6" minimum depth of footing, the elevation for the top of any footing may be raised. However, the top of the footing shall not be less than 1'-0" from the surface of the final ground elevation of the pier in question.
 In the case of overbreakage of ledge no payment will be made for rock excavation, concrete and cement of depths more than 6" below the elevation of the bottom of any column footing as previously determined by the Engineer.
 Minimum footing pressure = 14 tons / sq. ft.
 Reinforcing steel to have 2' Min. cover unless otherwise shown.



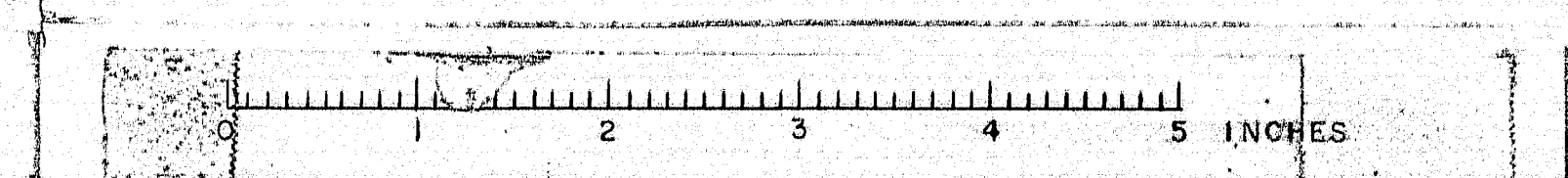
TABLE

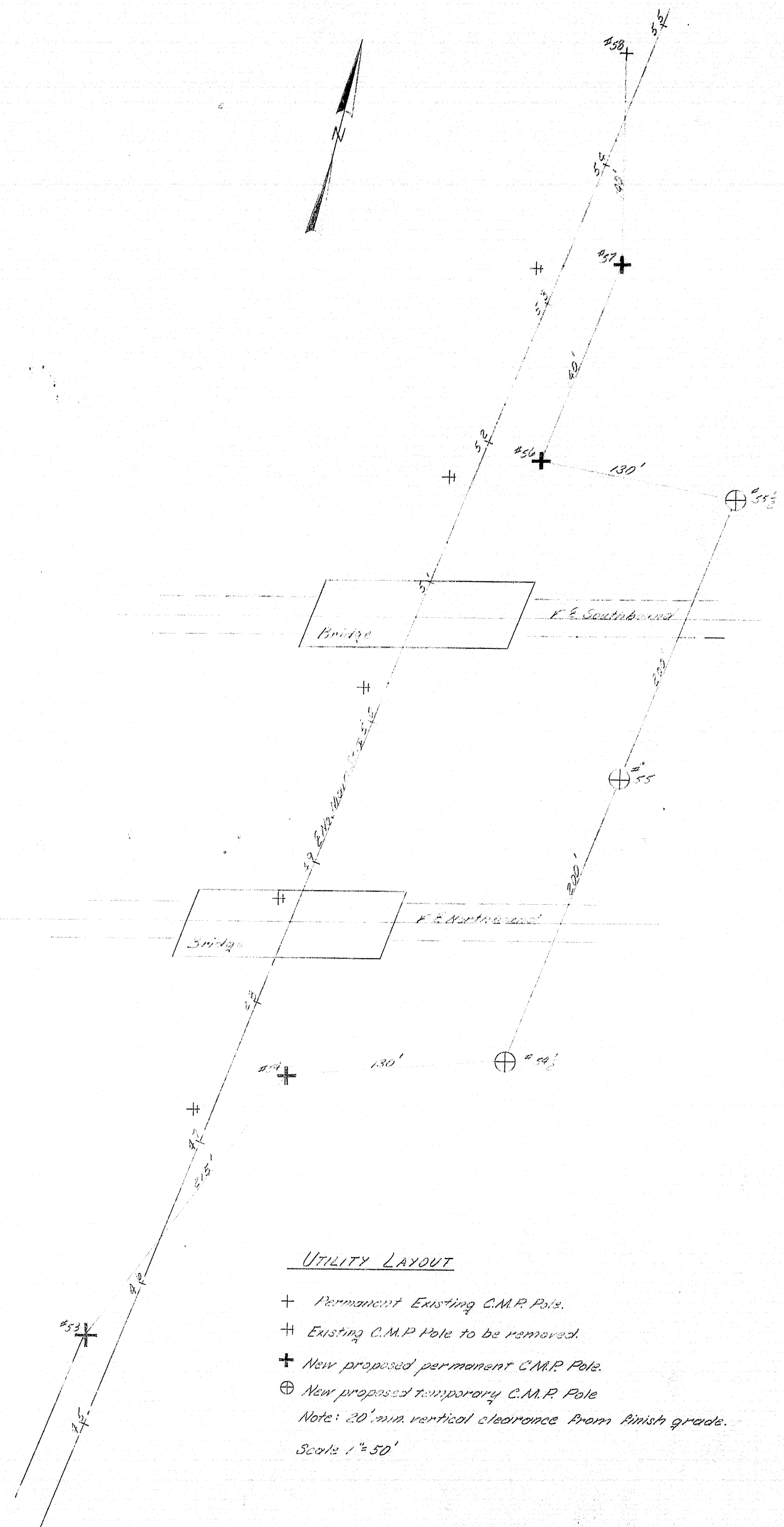
LANE	PIER	COLUMN	ELEV. A	ELEV. B	* APPROXIMATE C. BAR LENGTH * * ELEVATION *	NUMBER OF SI BARS *
SOUTHBOUND	1	I	276.11	278.0	275.0	18
		II	276.11	278.0	275.0	18
		III	276.11	278.0	275.0	18
SOUTHBOUND	2	I	276.46	281.0	278.0	16
		II	276.46	281.0	278.0	16
		III	276.46	281.0	278.0	16
NORTHBOUND	1	I	275.68	279.5	276.5	16
		II	275.68	279.5	276.5	16
		III	275.68	279.5	276.5	16
NORTHBOUND	2	I	276.23	280.0	277.0	16
		II	276.23	280.0	277.0	16
		III	276.23	280.0	277.0	16

* Because the ledge elevations may vary from those elevations listed above, C. Bars and SI Bars are not to be ordered until the ledge is exposed by structural excavation and a definite ledge elevation is established. Then, 1' of concrete, 6 Bar lengths and the number of SI Bars are to be ordered by the Engineer to fit the situation.

DESIGN: KHS
 CHECK: BLS
 STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
INTERSTATE 95
 OVER
NORTH MAIN STREET
 IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
 PIERS, NORTHBOUND AND SOUTHBOUND
 SHEET 14 OF 19 AUGUSTA, MAINE MARCH 1963

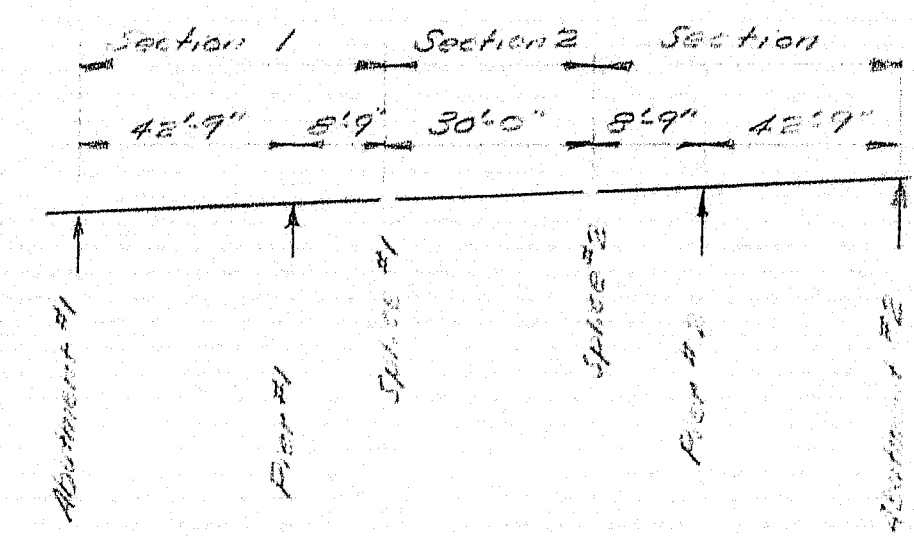
86-195





STRINGER SLOPES EXPRESSED IN PERCENT OF GRADE

Stringer	Northbound			Stringer	Southbound		
	1	2	3		1	2	3
a	1.3	1.1	0.91	a	0.58	0.60	0.42
b	1.3	1.1	0.91	b	0.82	0.63	0.44
c	1.3	1.1	0.94	c	0.91	0.61	0.41
d	1.3	1.2	0.94	d	0.74	0.61	0.47
e	1.3	1.2	0.94	e	0.74	0.73	0.49
f	1.4	1.1	0.98	f	0.94	0.73	0.49



GRADE DIAGRAM
Typical Both NB & SB

BRIDGE QUANTITIES		
ITEM	UNIT	QUANTITY
Structural Earth Excavation - Piers	CY	350
*Bituminous Concrete Surface Course - Type A	Tons	135
Portland Cement Concrete - Abuts & Retaining Walls	C.Y.	400
Portland Cement Concrete - Piers	C.Y.	195
Portland Cement Concrete - Rdwy & SW Slabs on Steel Br.	C.Y.	320
Portland Cement	bbls	1380
Structural Steel, Fabricated & Delivered	L.S.	L.S.
Structural Steel, Erection	L.S.	L.S.
Structural Steel, Field Painting	L.S.	L.S.
Reinforcing Steel, Delivered	Lbs	134,700
Reinforcing Steel, Placing	Lbs	134,700
Steel H-Beam, Piles, 12"/ft	Lin. Ft.	1358
Aluminum Rail - Alternate "A"	Lin. Ft.	606
Steel Rail - Alternate "B"	Lin. Ft.	606
*Membrane Waterproofing	S.Y.	1230
Epoxy Resin Surface Sealant	S.Y.	180
Granite Bridge Curb	Lin. Ft.	624

Note: The estimated weight of Structural Steel is 230,700 lbs, which includes drains.
*These items not a part of this contract.

UTILITY LAYOUT

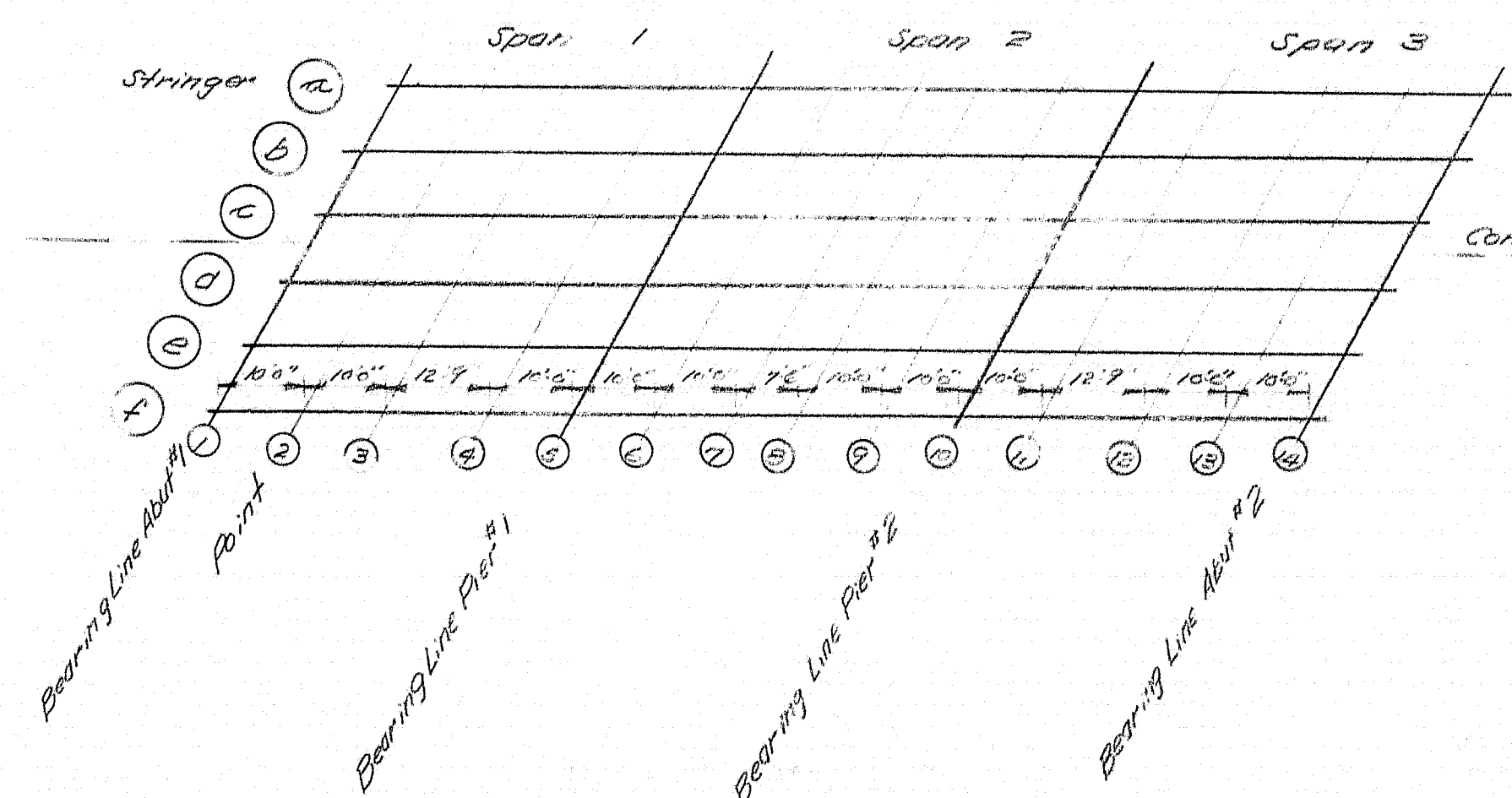
- + Permanent Existing C.M.P. Pole.
 - + Existing C.M.P. Pole to be removed.
 - + New proposed permanent C.M.P. Pole.
 - ⊕ New proposed temporary C.M.P. Pole
- Note: 20' min. vertical clearance from finish grade.
Scale: 1" = 50'

BLOCKING SCHEDULE

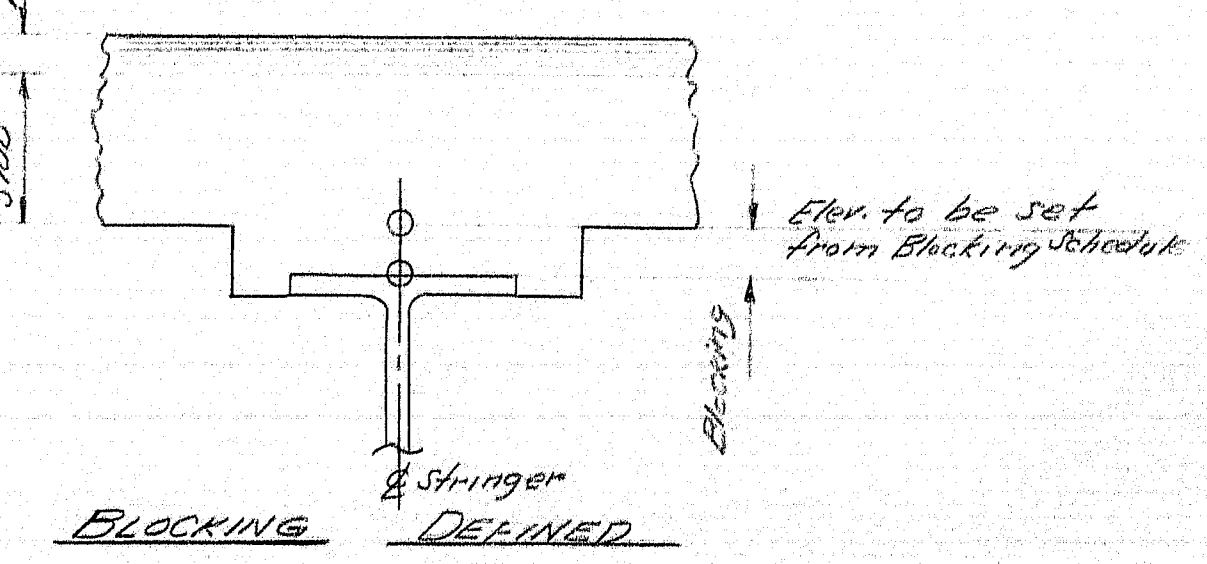
SOUTH BOUND		STRINGER					
SPAN	Point	a	b	c	d	e	f
1	1	302.76	302.86	302.96	303.00	302.83	302.67
	2	302.87	302.98	303.08	303.12	302.96	302.79
	3	302.97	303.08	303.19	303.23	303.06	302.89
	4	303.06	303.17	303.28	303.32	303.16	302.99
	5	303.13	303.24	303.35	303.40	303.23	303.07
2	6	303.20	303.32	303.43	303.47	303.31	303.15
	7	303.28	303.40	303.51	303.56	303.40	303.24
	8	303.33	303.45	303.56	303.61	303.45	303.29
	9	303.38	303.50	303.61	303.66	303.51	303.35
	10	303.43	303.55	303.67	303.72	303.57	303.42
3	11	303.49	303.62	303.74	303.79	303.64	303.48
	12	303.57	303.69	303.81	303.87	303.72	303.57
	13	303.60	303.72	303.85	303.91	303.76	303.61
	14	303.61	303.74	303.87	303.92	303.78	303.63

NORTH BOUND		STRINGER					
SPAN	Point	a	b	c	d	e	f
1	1	302.35	302.44	302.53	302.42	302.24	302.06
	2	302.51	302.60	302.69	302.58	302.40	302.22
	3	302.65	302.74	302.83	302.72	302.54	302.36
	4	302.79	302.89	302.98	302.87	302.70	302.52
	5	302.90	303.00	303.10	302.99	302.81	302.64
2	6	303.02	303.12	303.22	303.11	302.94	302.76
	7	303.14	303.24	303.34	303.24	303.06	302.89
	8	303.23	303.33	303.43	303.32	303.15	302.98
	9	303.32	303.43	303.53	303.43	303.26	303.08
	10	303.42	303.53	303.63	303.53	303.36	303.19
3	11	303.53	303.64	303.74	303.64	303.47	303.31
	12	303.66	303.77	303.88	303.78	303.62	303.45
	13	303.75	303.86	303.96	303.87	303.70	303.54
	14	303.81	303.92	304.03	303.93	303.77	303.61

Note: In order to compensate for dead load deflections and normal irregularities in the stringers, produced in the rolling process, set the elevations in the table above at the points indicated below before any of the slab forms are started.



BLOCKING DIAGRAM



BLOCKING DEFINED

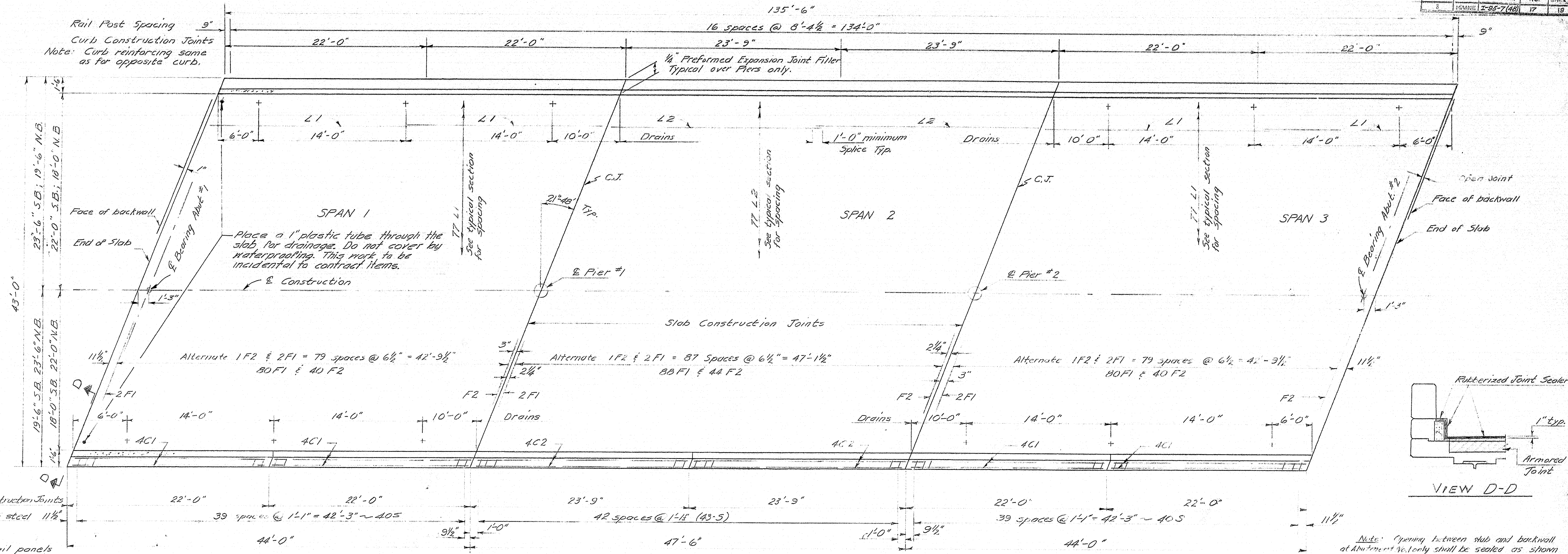
DESIGN - T.H.K. & A.L.L.
DETAIL - ALYON
CHECK - [Signature]

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY

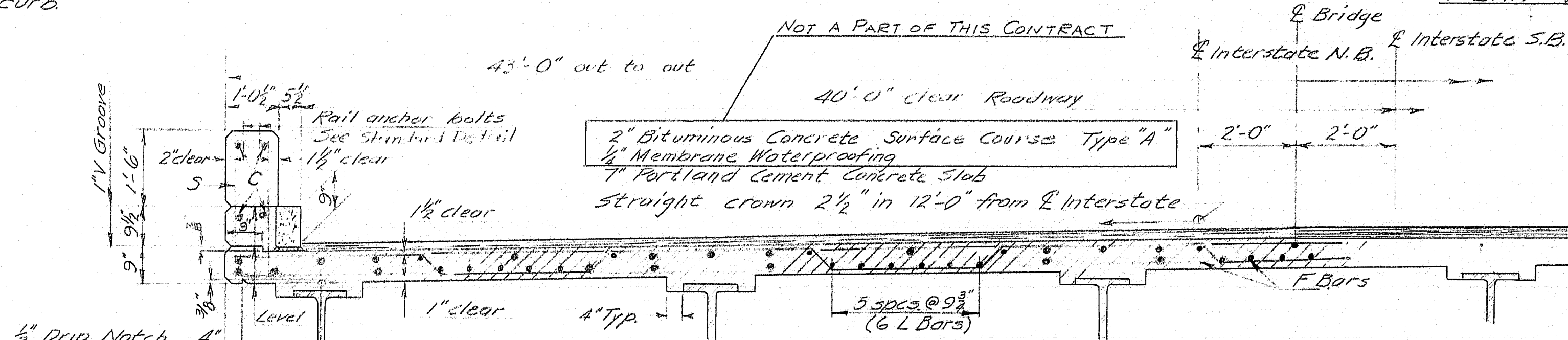
BLOCKING SCHEDULE
SHEET 16 OF 19 AUGUSTA, MAINE MARCH 1963

Rail Post Spacing
Curb Construction Joints
Note: Curb reinforcing same as for opposite curb.



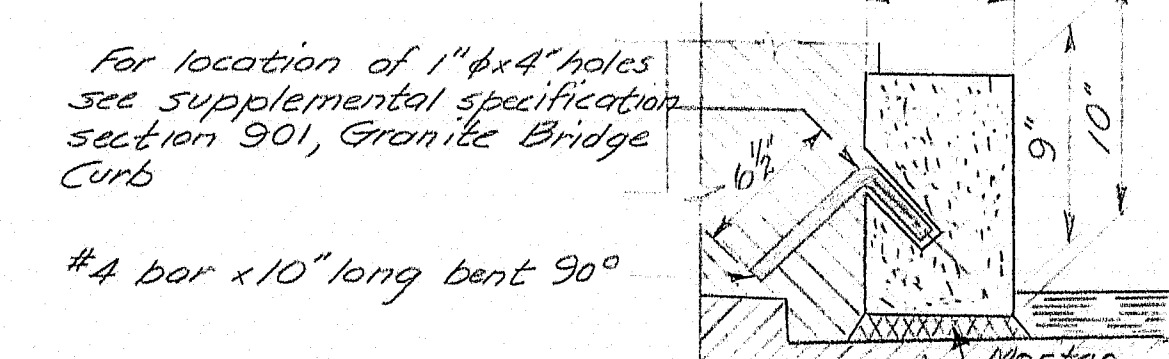
PLAN VIEW

Note: Drains shall be paid for under Items 702-103, 104, 104 & 702-105, Structural Steel



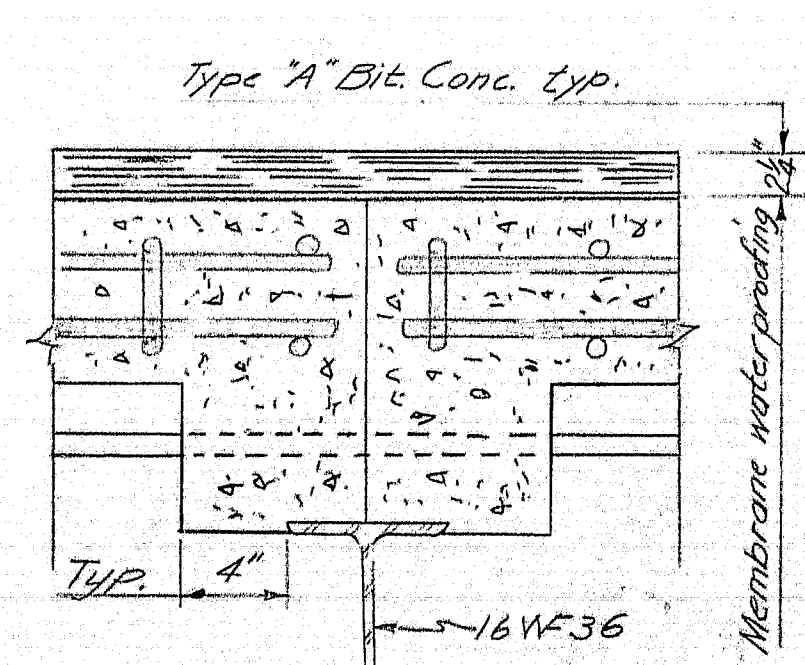
PARTIAL SECTION

Note: 36" minimum width 4x4-10 ga. galvanized welded wire mesh or equivalent acceptable to Field Engineer. Payment for furnishing and placing same to be incidental to Bituminous Concrete Surface Course Type "A"

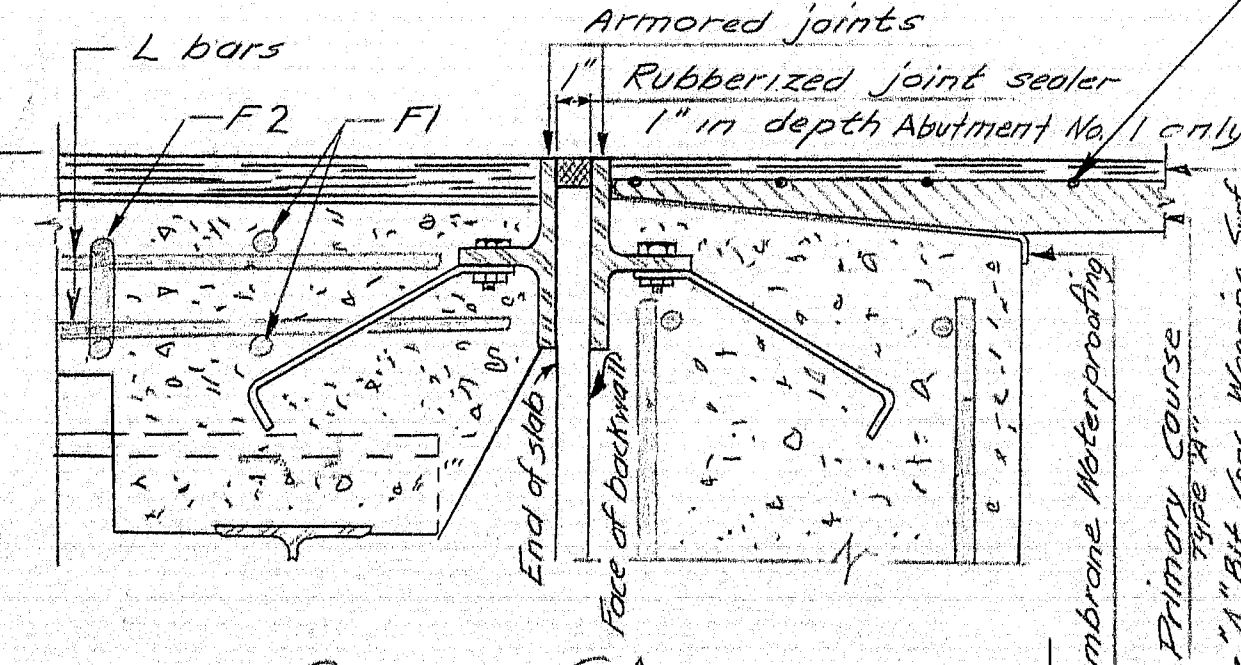


GRANITE BRIDGE CURB DETAIL

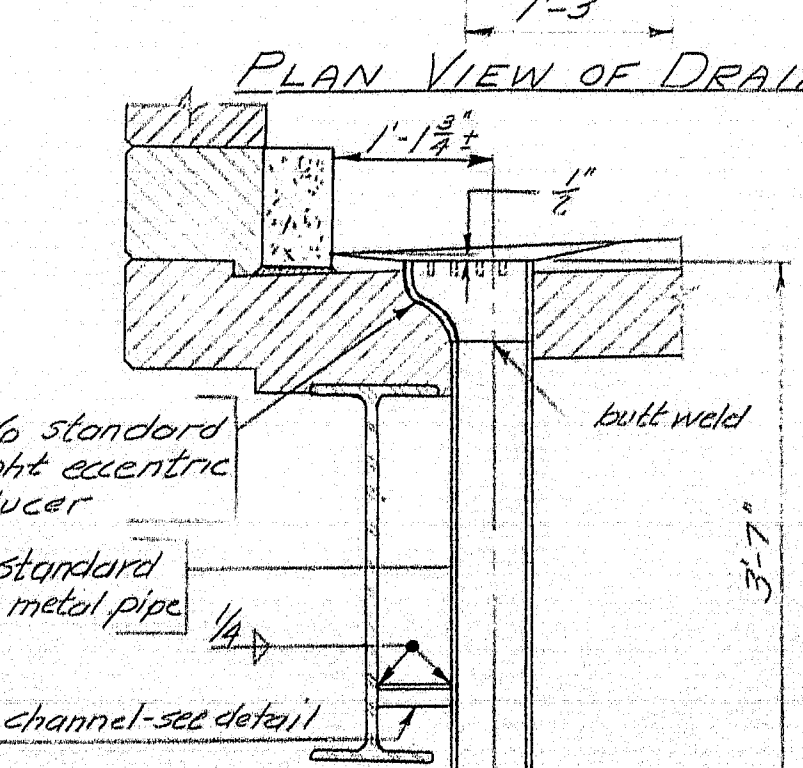
Note: Stone shall be cut square at curb construction joints and on a 22° skew at slab construction joints and at abutment backwalls.



TYPICAL SLAB CONSTR. JOINT



SECTION @ ABUTMENTS



PLAN VIEW OF DRAIN

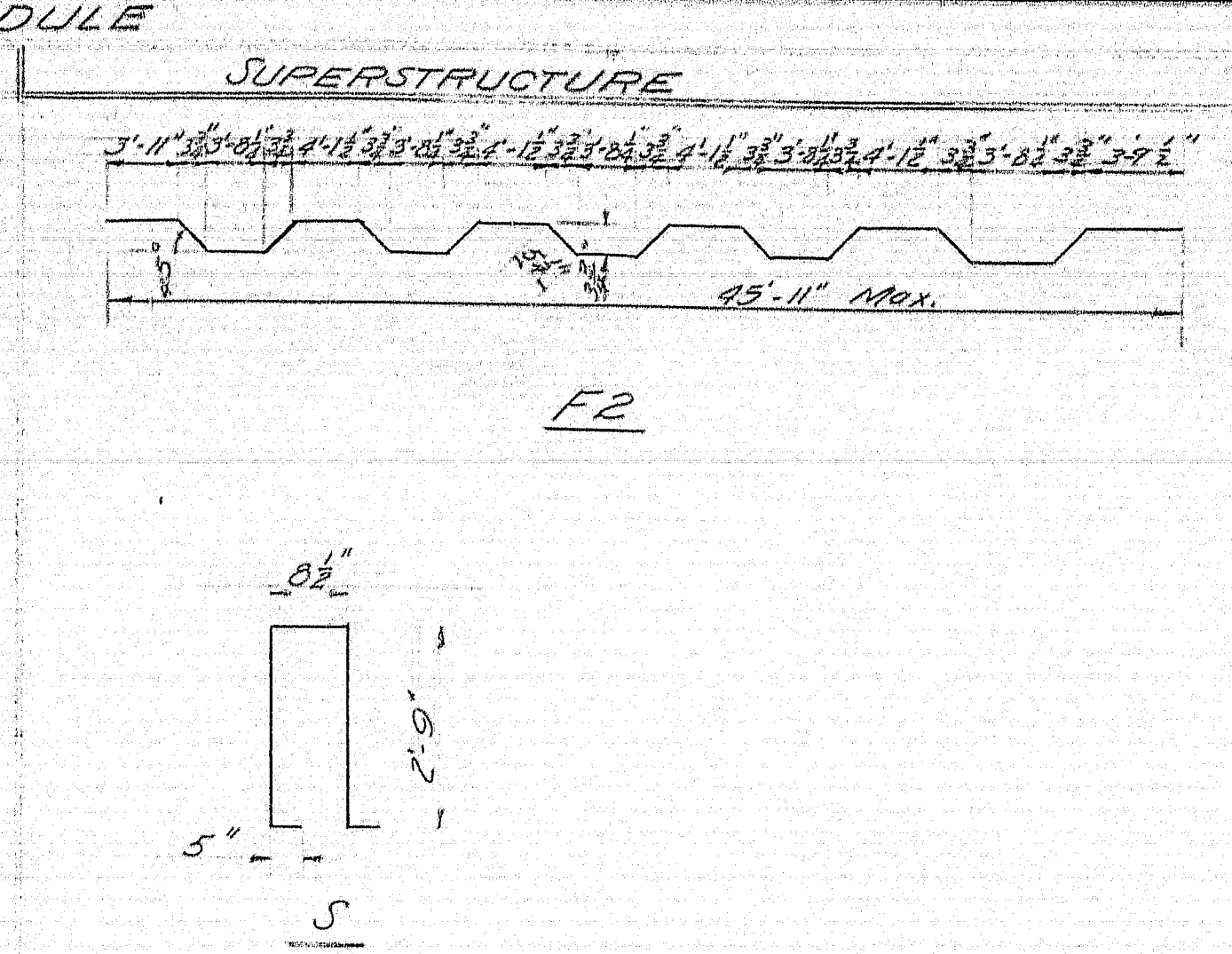
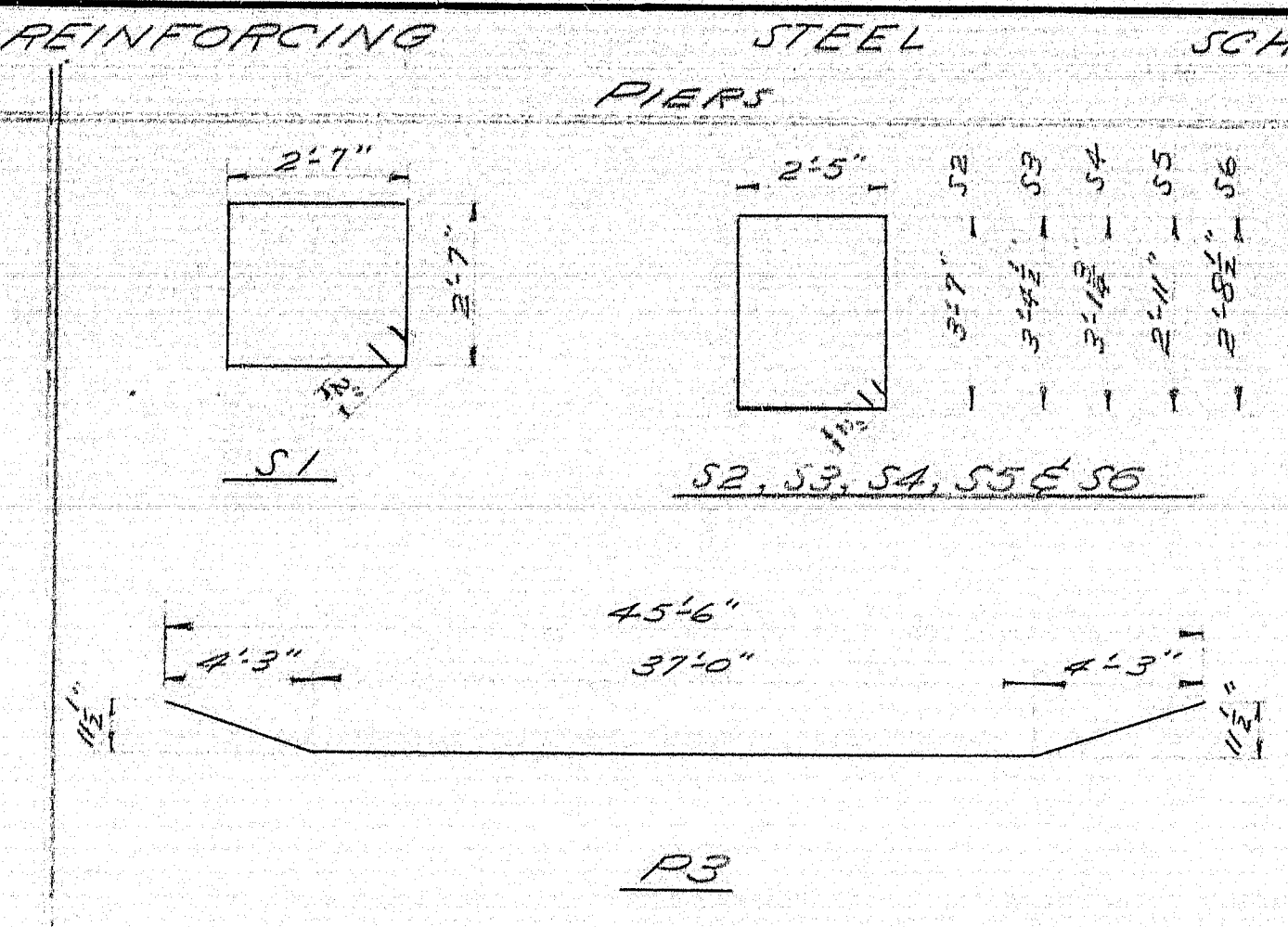
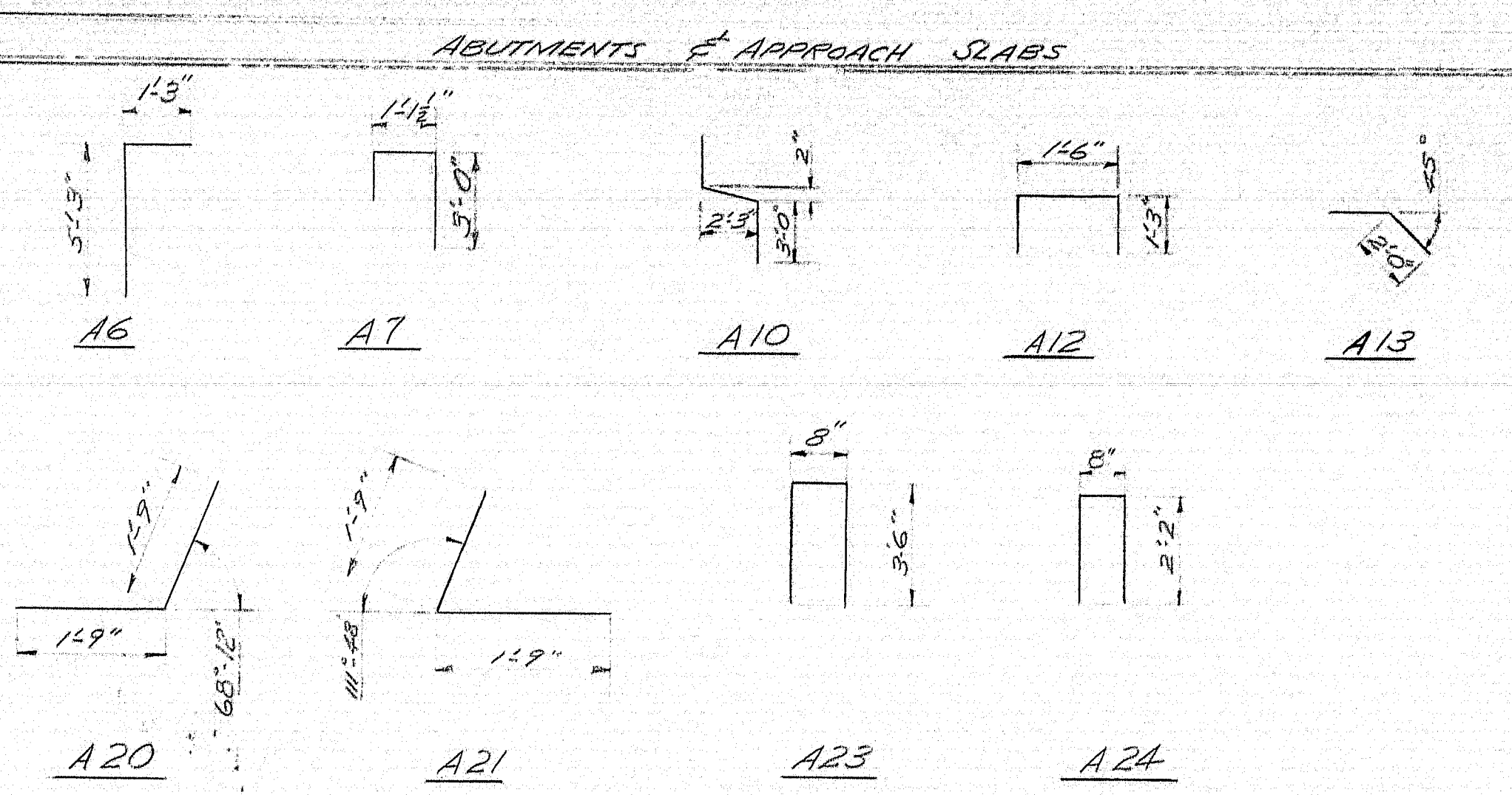
SECTION A-A

DESIGN: T.H.K., DET. H.F.
CHECK: B.G.M.

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
SUPERSTRUCTURE

SHEET 17 OF 19 AUGUSTA, MAINE MARCH 1963



Bar	Size	Number	Length	Location
A6	#5	176	6'6"	Bridge Seats
A7	5	111	5'9"	Back walls
A10	5	116	7'0"	Bridge Seat
A12	5	72	4'0"	Bearing Areas
A13	5	112	3'0"	Approach Slab
A20	6	32	3'6"	South Wing
A21	6	36	3'6"	North Wing
A23	4	80	7'5"	Wings
A24	#4	24	5'0"	End Posts

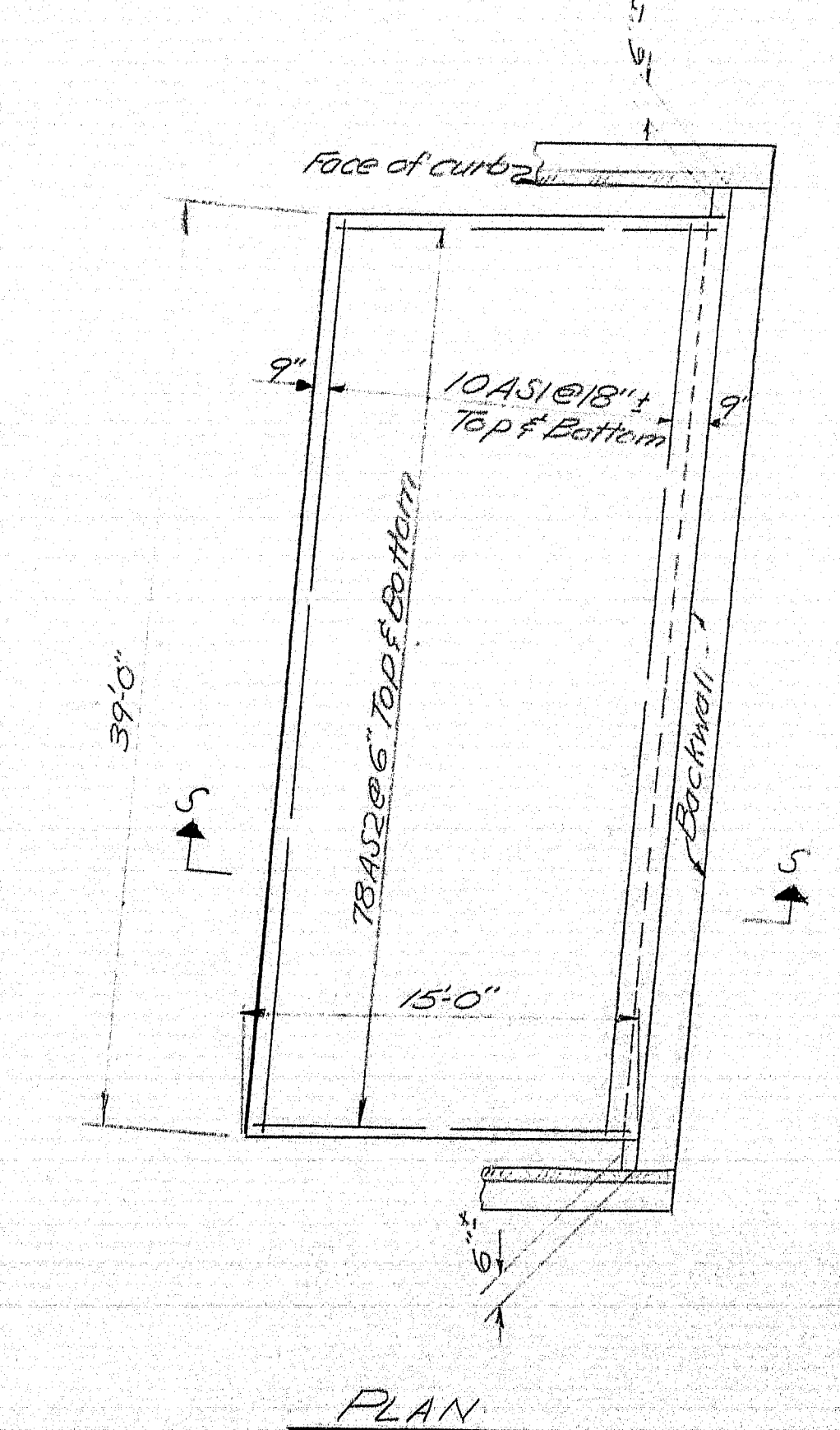
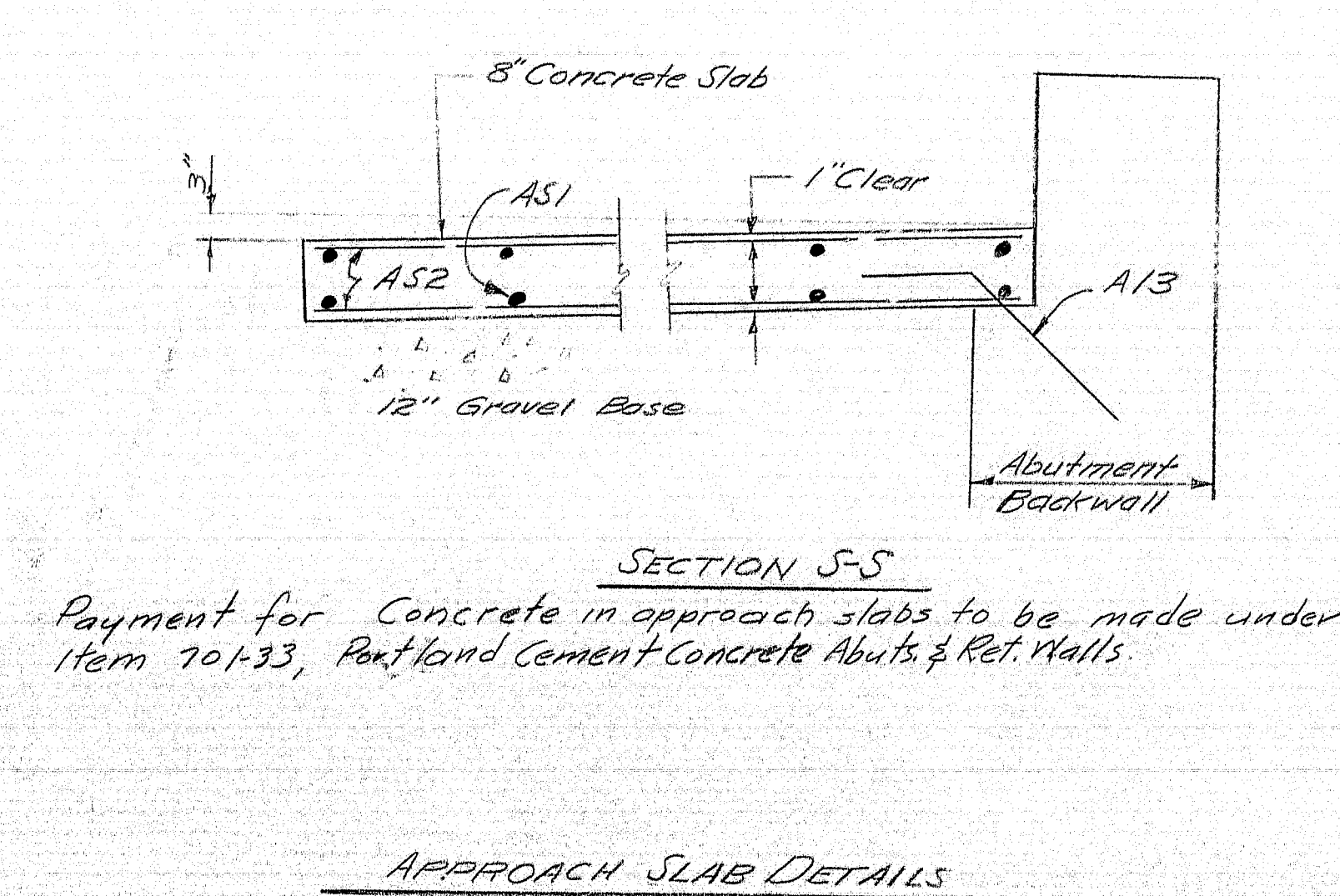
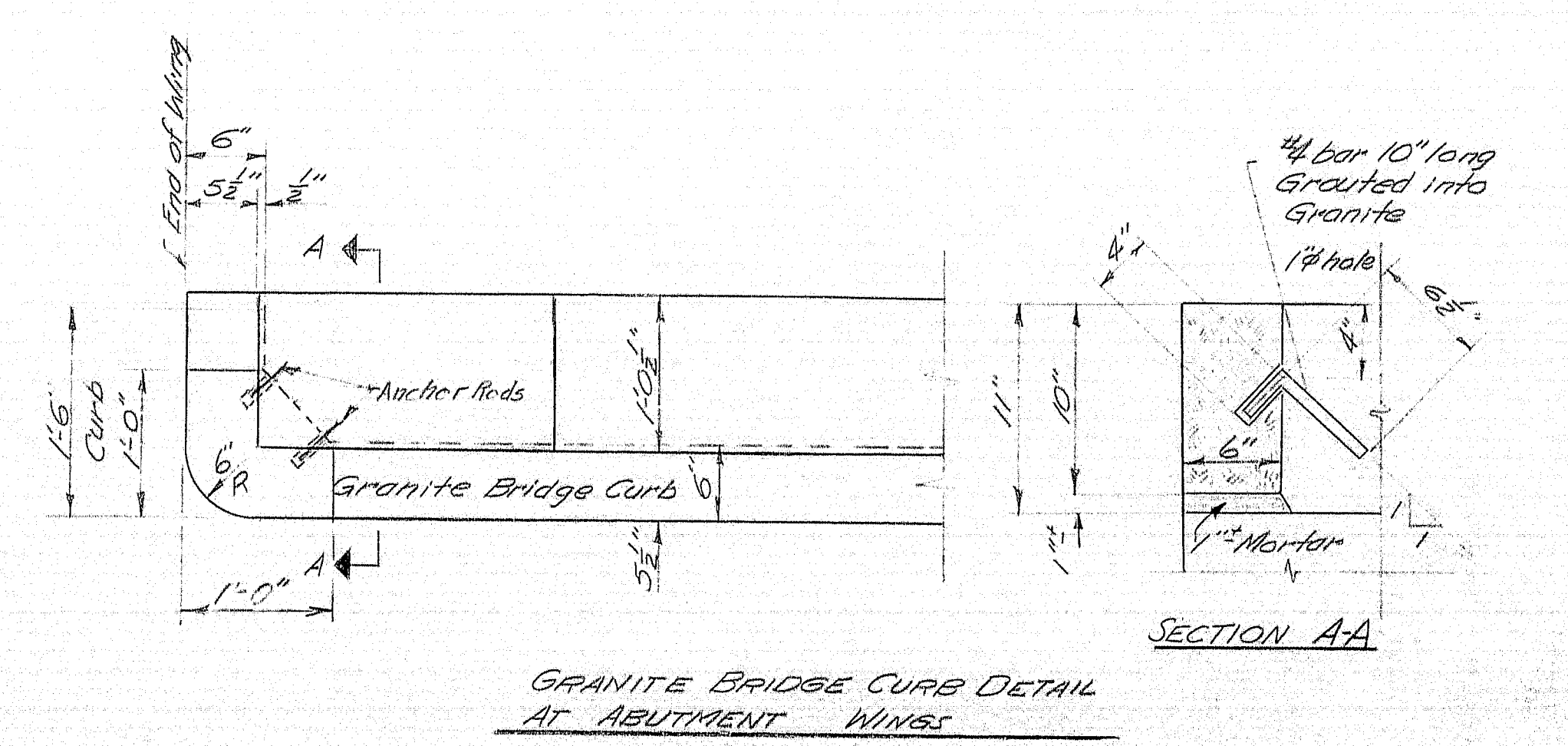
Bar	Size	Number	Length	Location
A1	#6	112	24'9"	Footings
A2	6	336	6'0"	Footings
A3	6	64	13'9"	Footings
A4	6	120	3'0"	Footings
A5	5	372	3'0"	Footings to Br. Seat
A8	4	48	25'6"	Br. Seat & Bk. Wall
A9	4	48	21'0"	Br. Seat & Bk. Wall
A11	5	72	2'5"	Bearing Areas
A14	5	80	5'2"	Wings
A15	5	160	3'6"	Wings
A16	6	80	3'0"	Wings
A17	6	80	5'4"	Wings
A18	4	48	9'6"	Wings
A19	6	72	9'6"	Wings
A22	4	32	9'0"	Wings
A25	#4	32	1'8"	End Posts
AS1	4	80	38'6"	Approach Slabs
AS2	6	624	14'6"	Approach Slabs
A26	4	16	9'6"	Wings

Bar	Size	Number	Length	Location
S1	#4	195	10'8"	All Pier Columns
S2	4	160	12'4"	Pier Caps
S3	4	16	11'11"	Pier Caps
S4	4	16	11'6"	Pier Caps
S5	4	16	11'0"	Pier Caps
S6	4	16	10'7"	Pier Caps
P3	#10	12	45'8"	Pier Caps

Bar	Size	Number	Length	Location
P1	#10	20	45'6"	Pier Caps
P2	6	16	45'6"	Pier Caps
P4	10	8	34'0"	Pier Caps
P5	6	240	4'6"	Footings
P6	8	192	4'6"	Dowels - Footings to Columns
C1	8	16	16'6"	N.B. Pier 1
C2	8	96	17'6"	N.B. Pier 2 Col 1 & 2
C3	8	32	19'0"	N.B. Pier 1 & 2
C4	8	48	20'0"	S.B. Pier 1

Bar	Size	Number	Length	Location
F2	6	545	47'5"	Adwy. Slab
S	4	472	7'0"	Curb Parapets

Bar	Size	Number	Length	Location
C1	#4	64	21'8"	Curb Parapet
C2	4	32	23'5"	Curb Parapet
F1	6	496	45'11"	Adwy. Slab
L1	6	308	22'4"	Adwy. Slab
L2	6	154	24'1"	Adwy. Slab

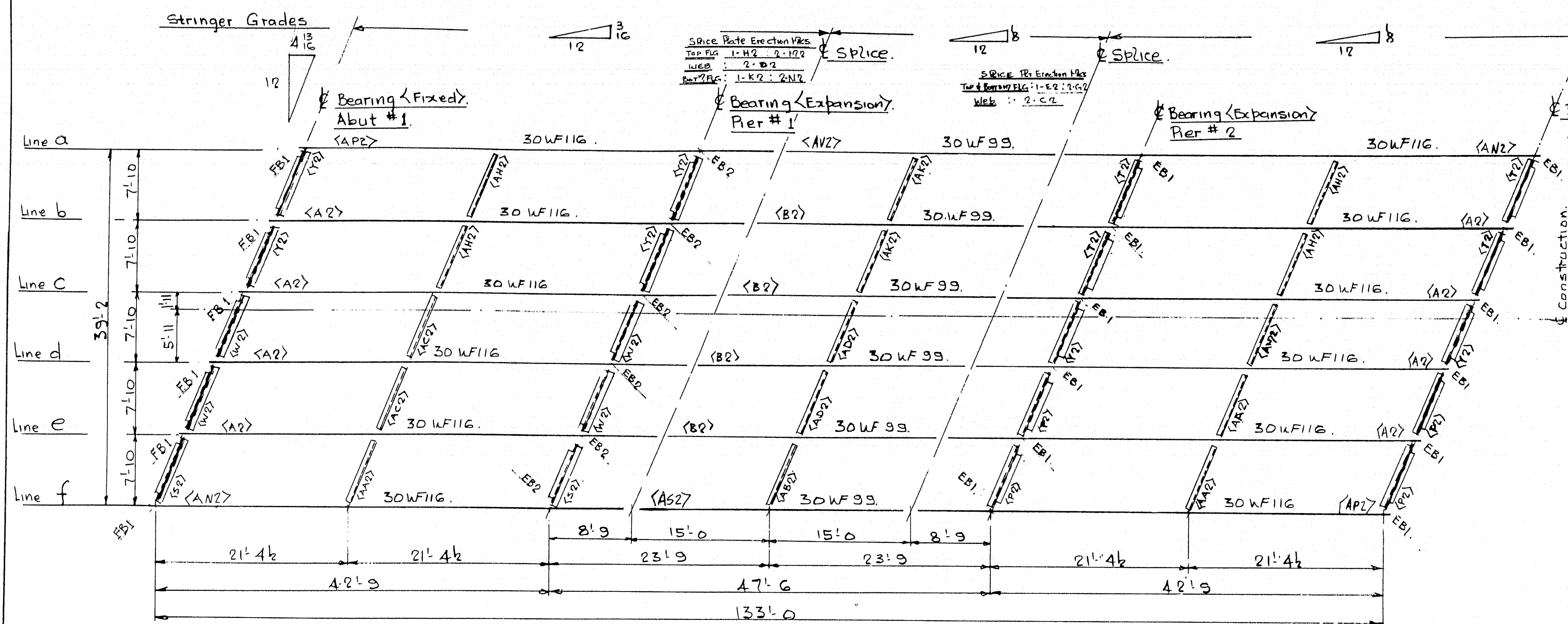


-Note-

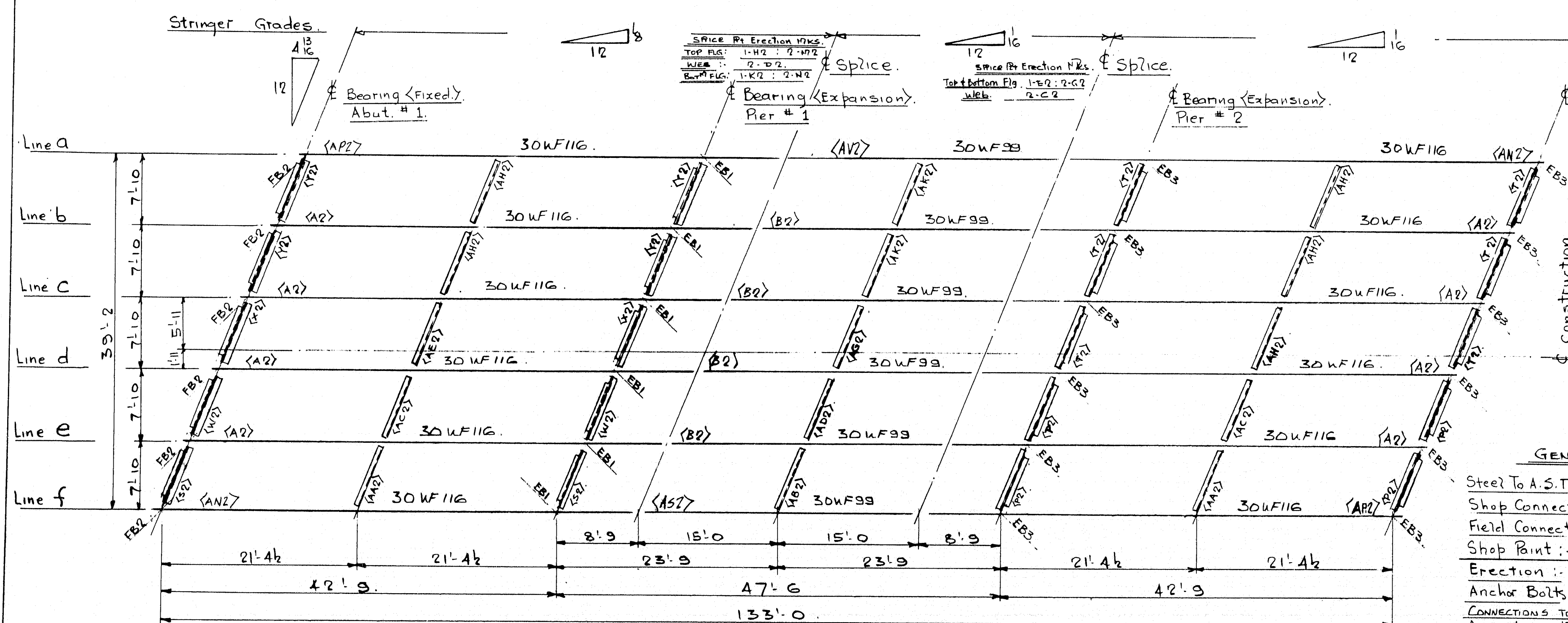
- 1) Reinforcing Steel to be of Intermediate Grade
- 2) Dimensions are to c. of Bars

DESIGN - T.H.K.
DETAIL - ALYON
CHECK - ALYON

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
REINFORCING STEEL - GRANITE BRIDGE CURB
APPROACH SLAB DETAILS
SHEET 19 OF 19 AUGUSTA, MAINE MARCH 1963

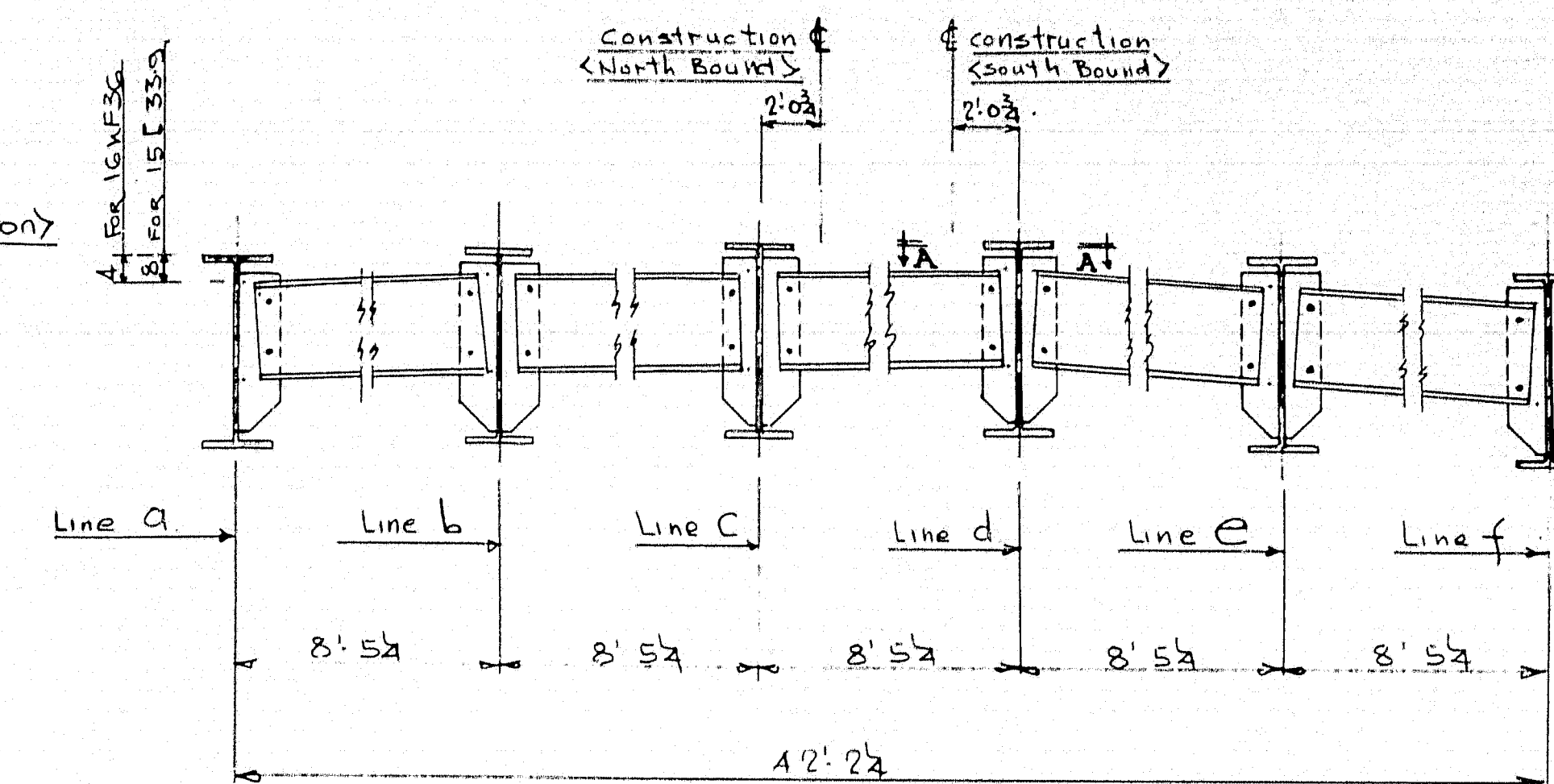


ERECTION DIAGRAM (North Bound)

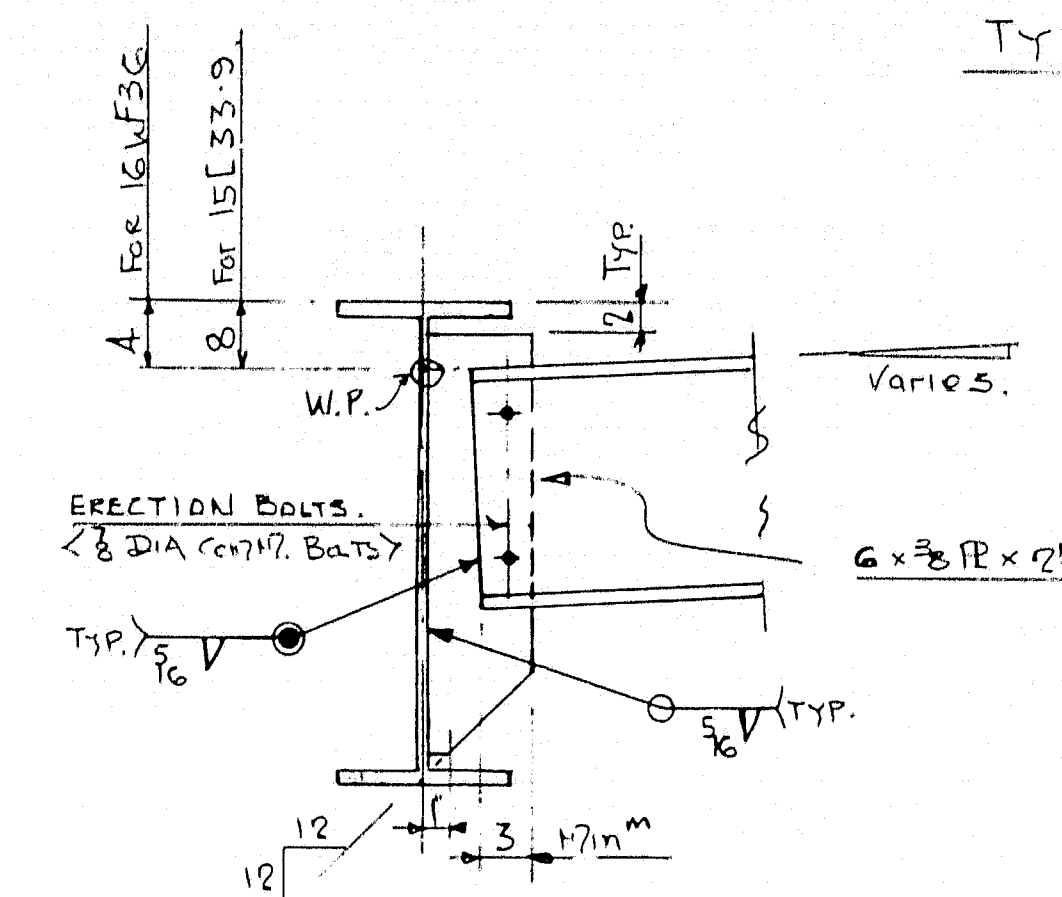


ERECTION DIAGRAM (South Bound)

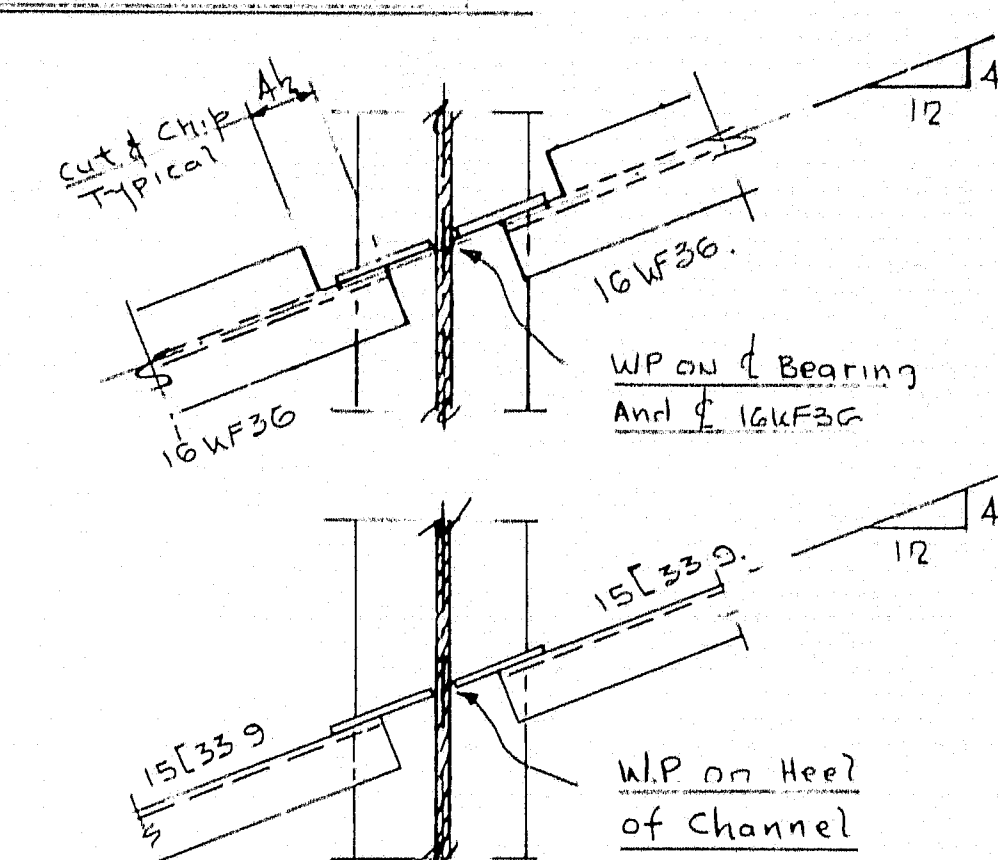
DIAPHRAGMS AT BEARINGS 16WF36.
INTERMEDIATE DIAPHRAGMS 15I33.9.
DIAPHRAGM SPACING DIMENSIONS ARE
TO 16WF36 & BACK OF 15I33.9.



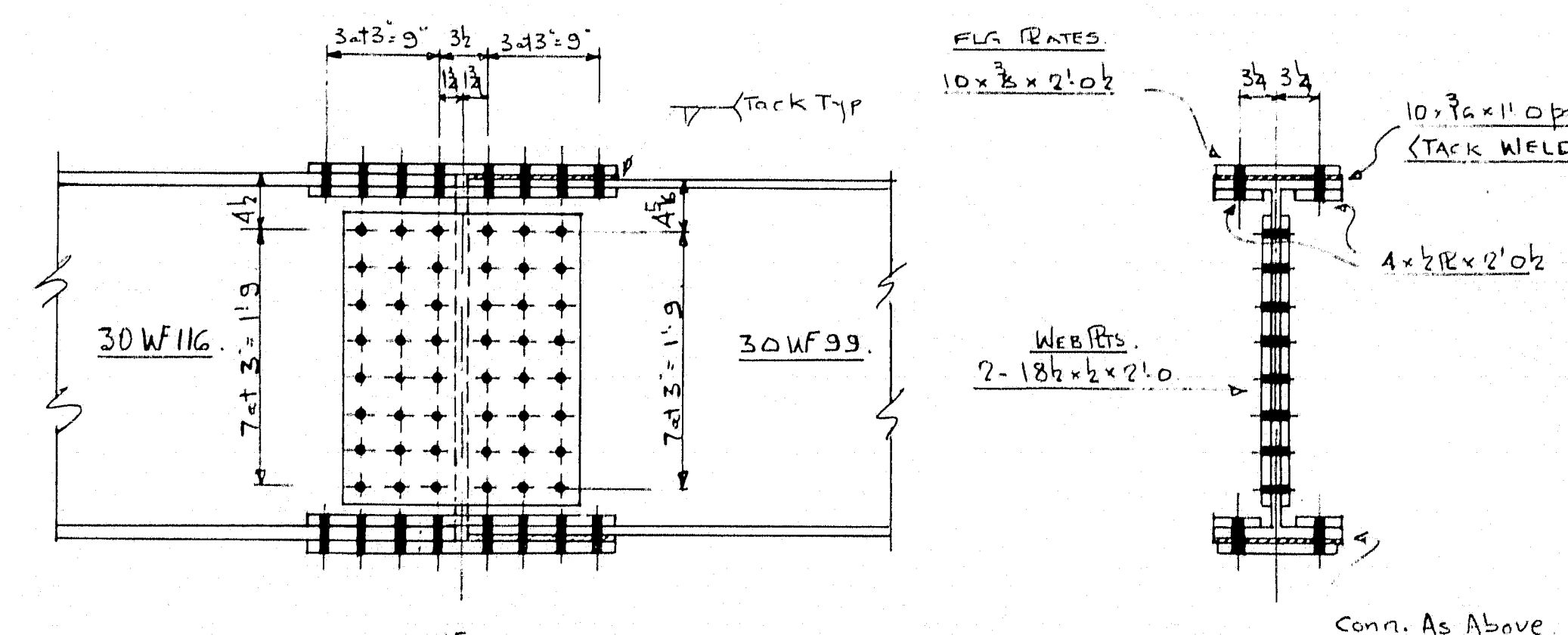
TYPICAL CROSS SECTION



TYPICAL DIAPHRAGM CONNECTION



PLAN ON A-A



TYPICAL SPICE DETAIL

GENERAL NOTES

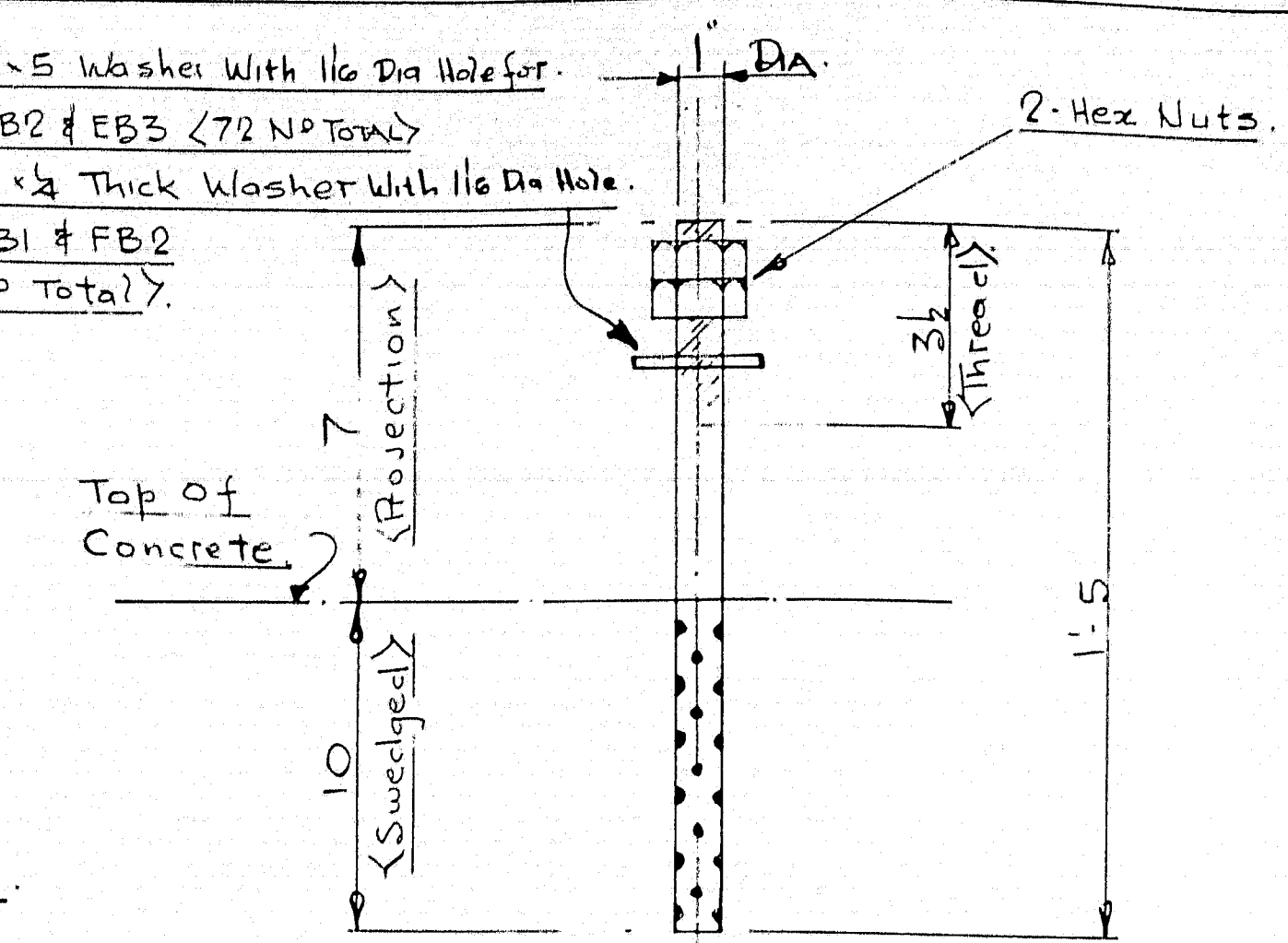
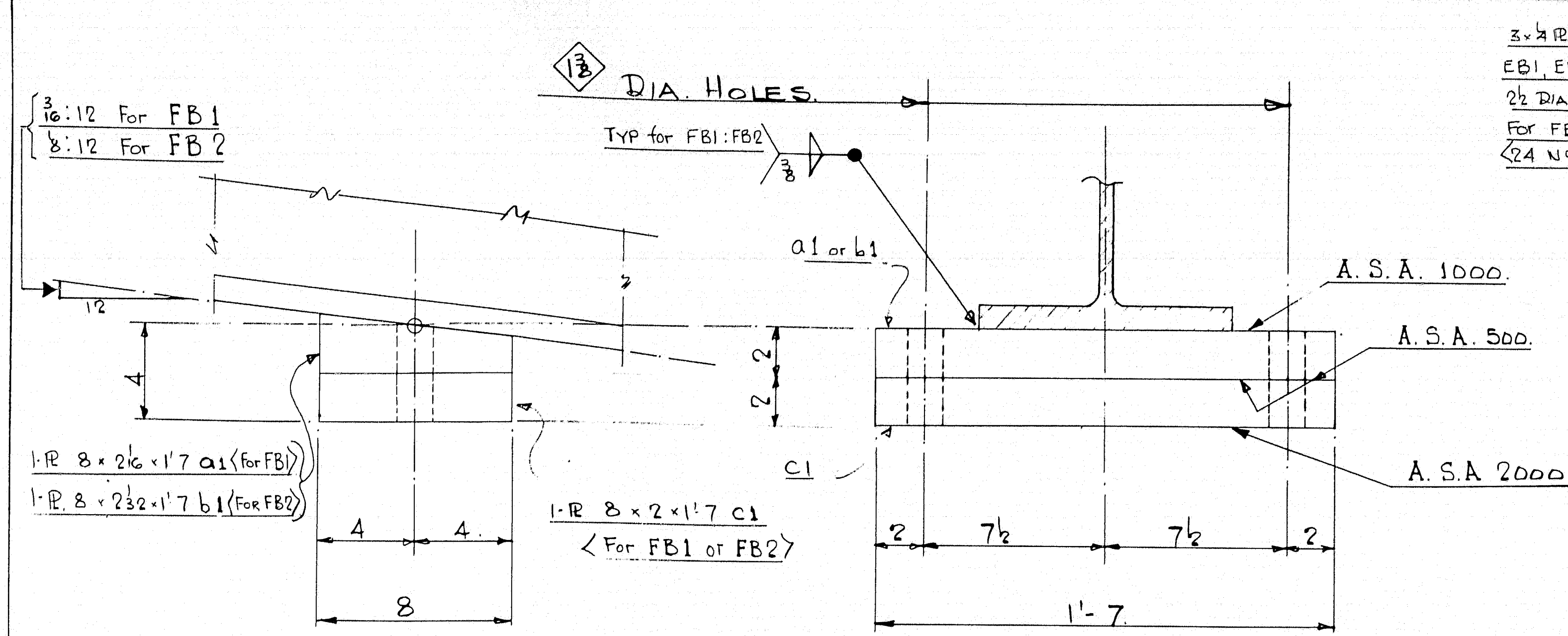
Steel To A.S.T.M. A36 (Latest Revision) See Specification.
Shop Connections: Welded.
Field Connections: Welded & H.T. Bolts.
Shop Paint: STD. RED PRIMER UN.
Erection: By Others.
Anchor Bolts: Supplied By A.I.W. Set By Others.
Connections To Be SUB-PUNCHED 1/4" AND REAMED
ASSEMBLED TO 1/4" OR DRILLED ASSEMBLED TO 1/4"

ERECTOR NOTE

Members To Be Erected So That
Marked End Is In Same Location
As Shown on This Drawing.

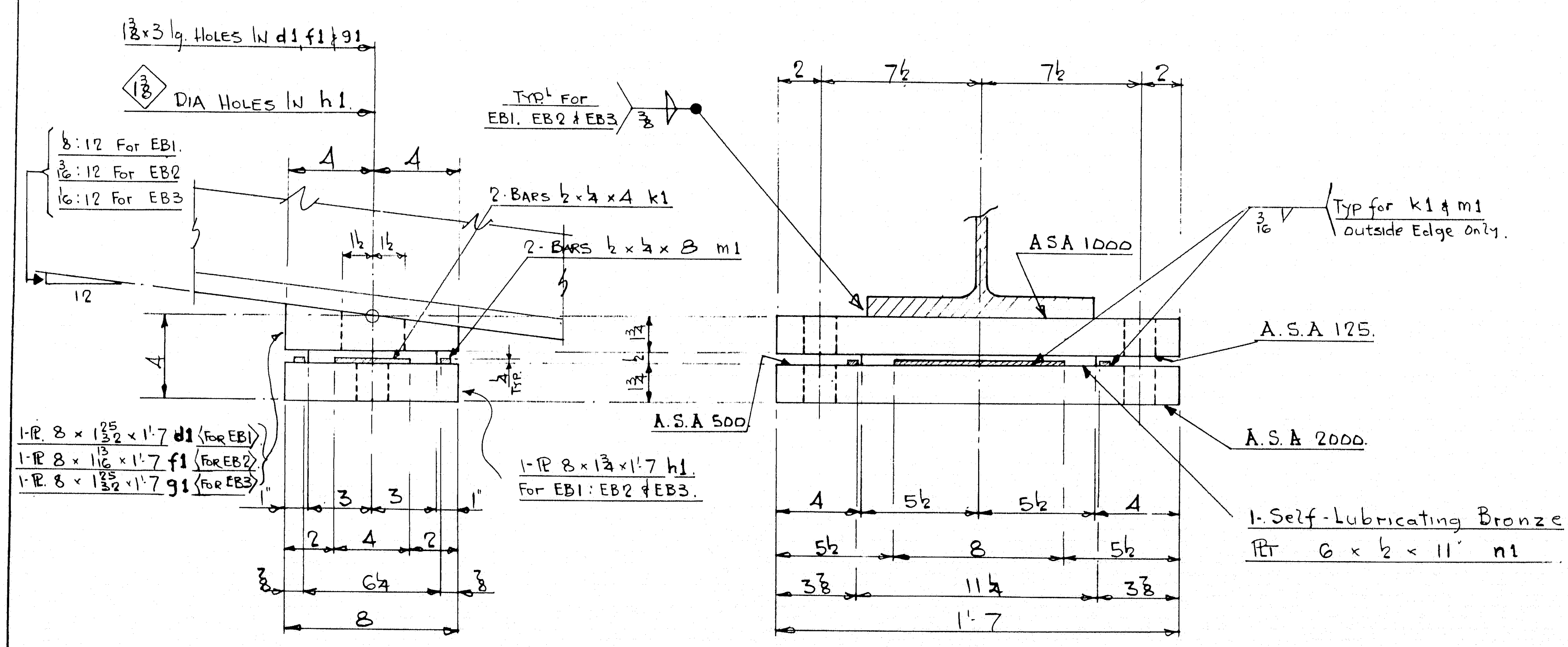
REVISIONS	DESCRIPTION	DATE
A	JOB: BRIDGE OVER NORTH MAIN STREET	
B	PITTSFIELD - MAINE	
C	ARCHT. STATE OF MAINE BRIDGE DEPT.	
D	CUSTOMER: REED & REED	
E	BATH - MAINE	
F	AUGUSTA IRON WORKS	
G	AUGUSTA, MAINE	
H		
J		

86-200A



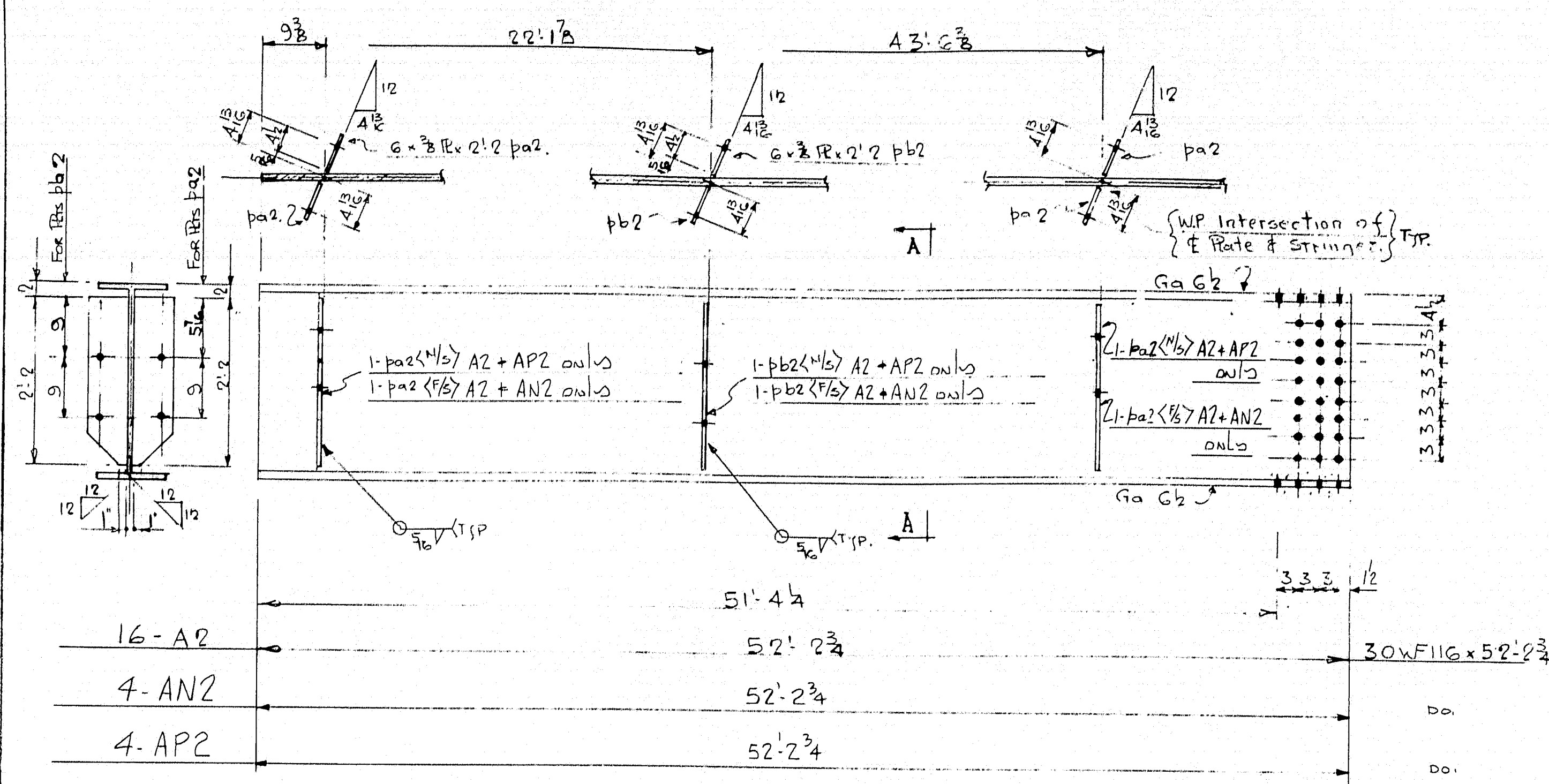
72 - ANCHOR BOLTS THUS WITH RT WASHER
24 - ANCHOR BOLTS THUS WITH RD WASHER

6 - FIXED BEARINGS - FB1 {Location: Lines a:b:c:d:e:f Abut #1 (North Bound)}
6 - FIXED BEARINGS - FB2 {Location: Lines a:b:c:d:e:f Abut #1 (South Bound)}

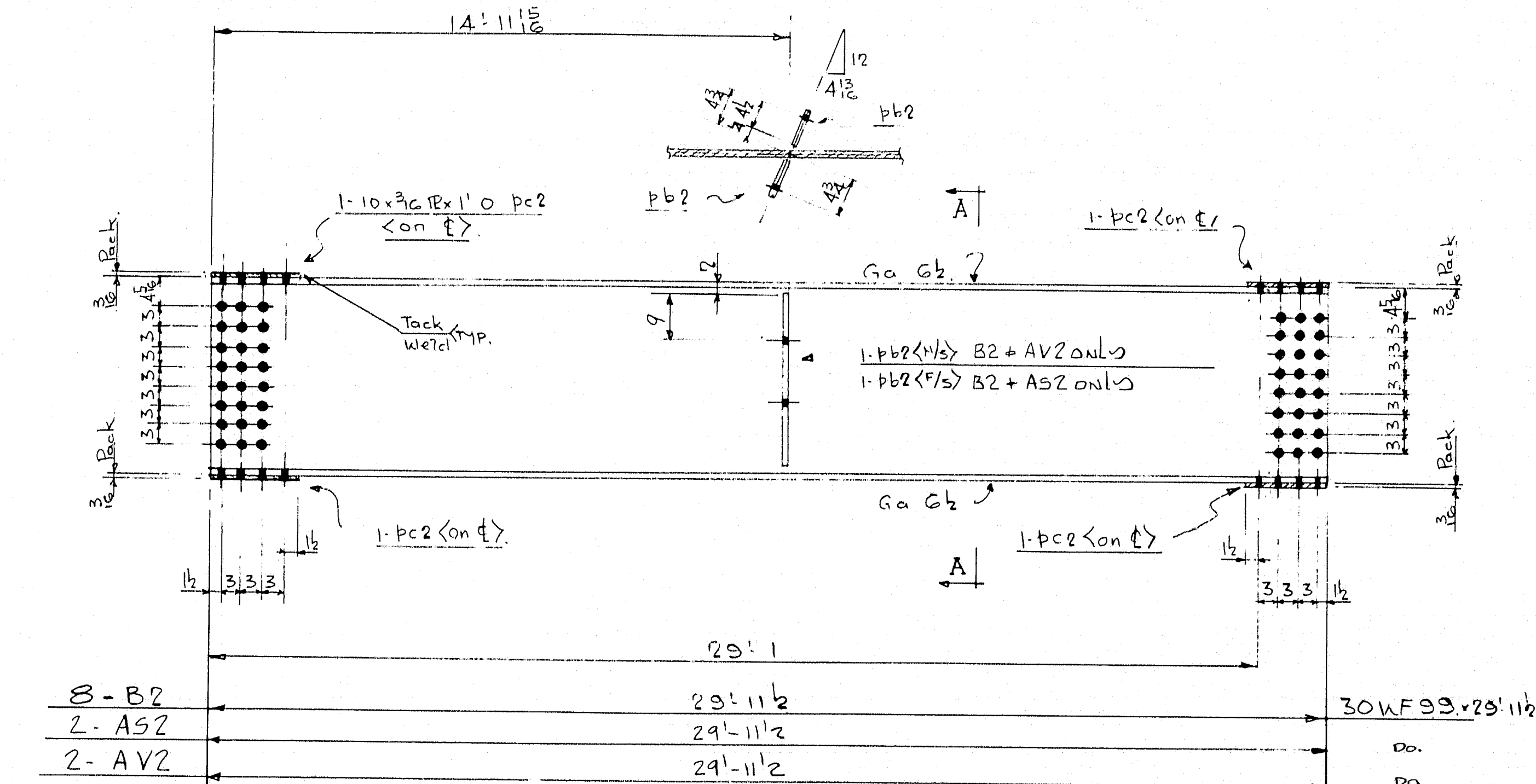


18 - EXPANSION BEARINGS EB1 {Location: Lines a:b:c:d:e:f Abut #2 North Bound
Lines a:b:c:d:e:f Pier #2 North Bound
Lines a:b:c:d:e:f Pier #1 South Bound}
6 - EXPANSION BEARINGS EB2 {Location: Lines a:b:c:d:e:f Pier #1 North Bound}
12 - EXPANSION BEARINGS EB3 {Location: Lines a:b:c:d:e:f Pier #2 South Bound
Lines a:b:c:d:e:f Abut #2 South Bound}

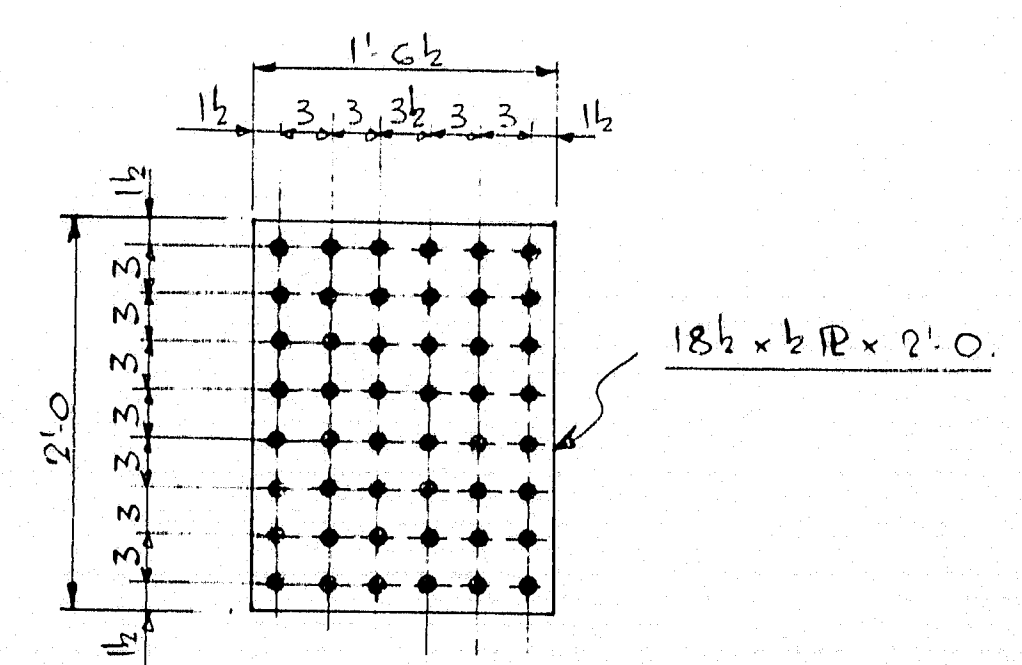
REVISIONS	DESCRIPTION	BEARING DETAILS
A	JOB:	BRIDGE OVER NORTH MAIN STREET
B		PITTSFIELD MAINE
C	ARCHT. STATE OF MAINE BRIDGE DEPT.	
D	CUSTOMER:	REED & REED BATH - MAINE
E		
F		
G		
H		
J		



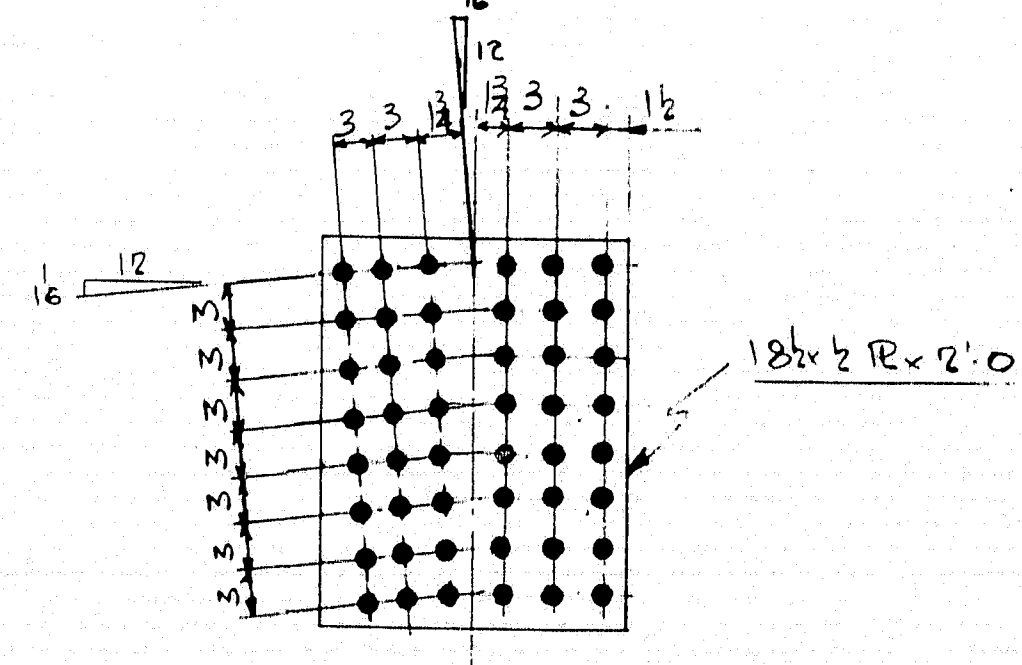
16-A2	51'-4 1/4"	30x116x52'-2 3/4"
4-AN2	52'-2 3/4"	DO
4-AP2	52'-2 3/4"	DO



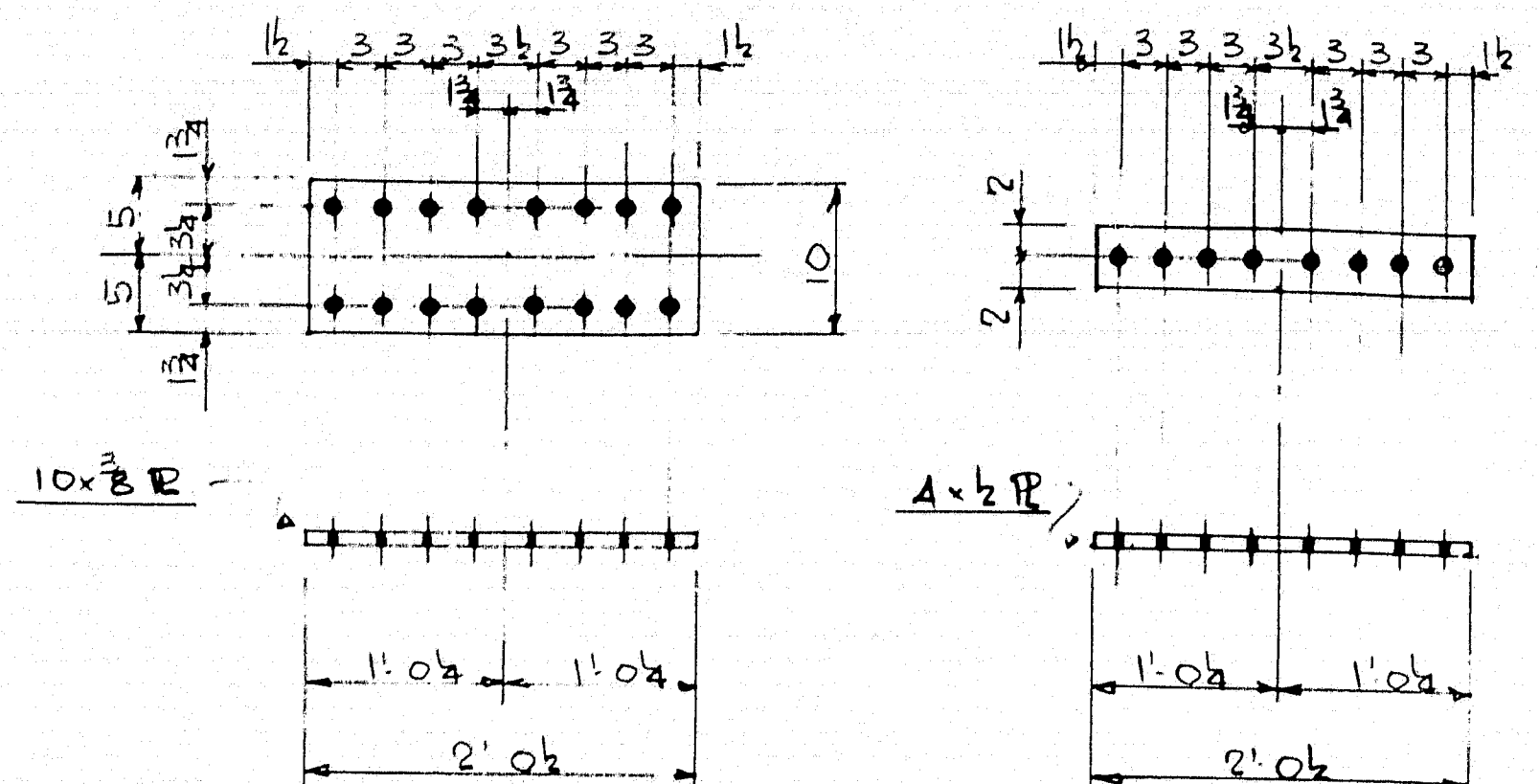
8-B2	29'-11 1/2"	30x116x29'-11 1/2"
2-AS2	29'-11 1/2"	DO
2-AV2	29'-11 1/2"	DO



24-SPLICE RTS THUS 1X P C2.



24-SPLICE RTS THUS 1X P D2.

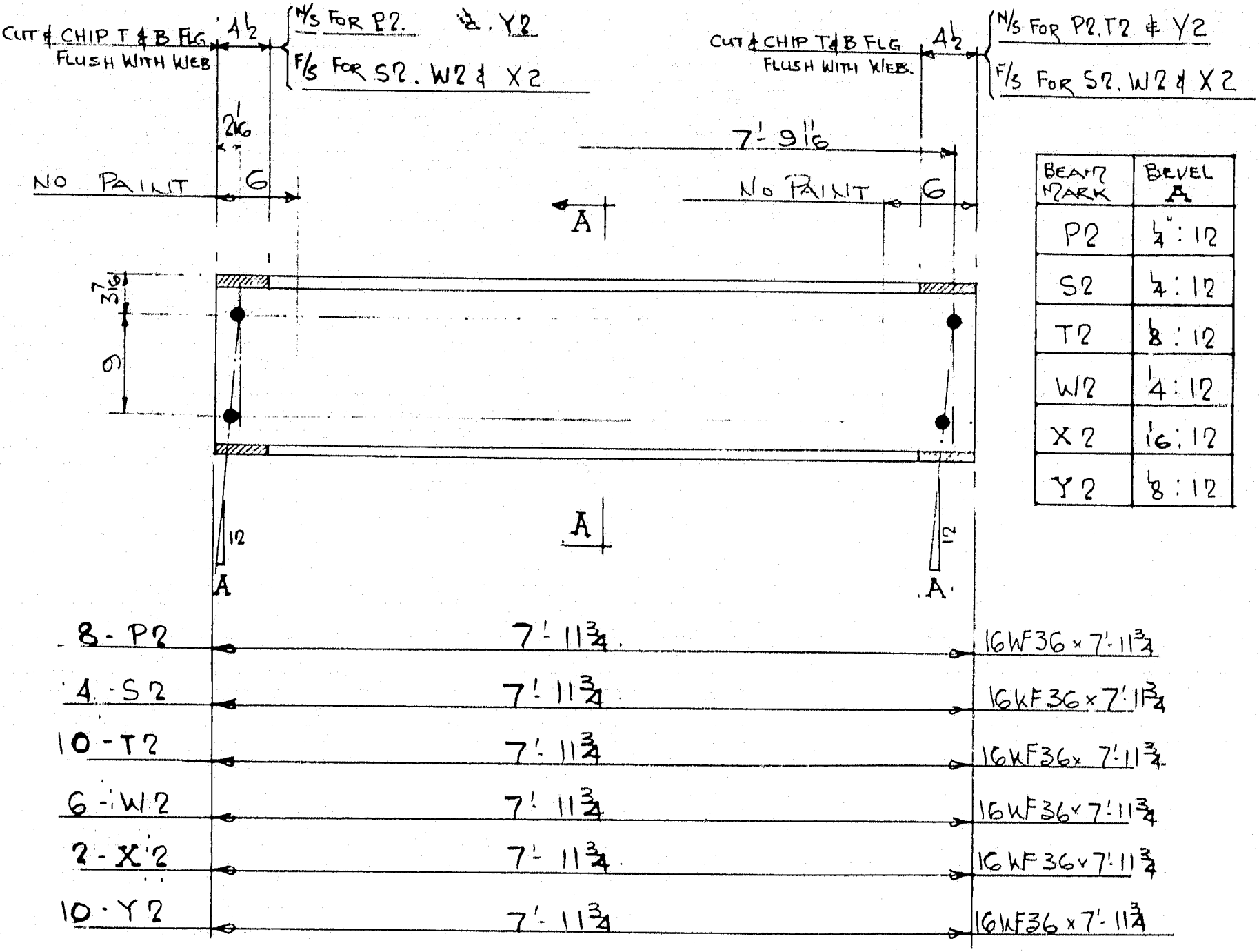


24-SPLICE RTS THUS 1X P E2

48-SPLICE RTS THUS 1X P G2

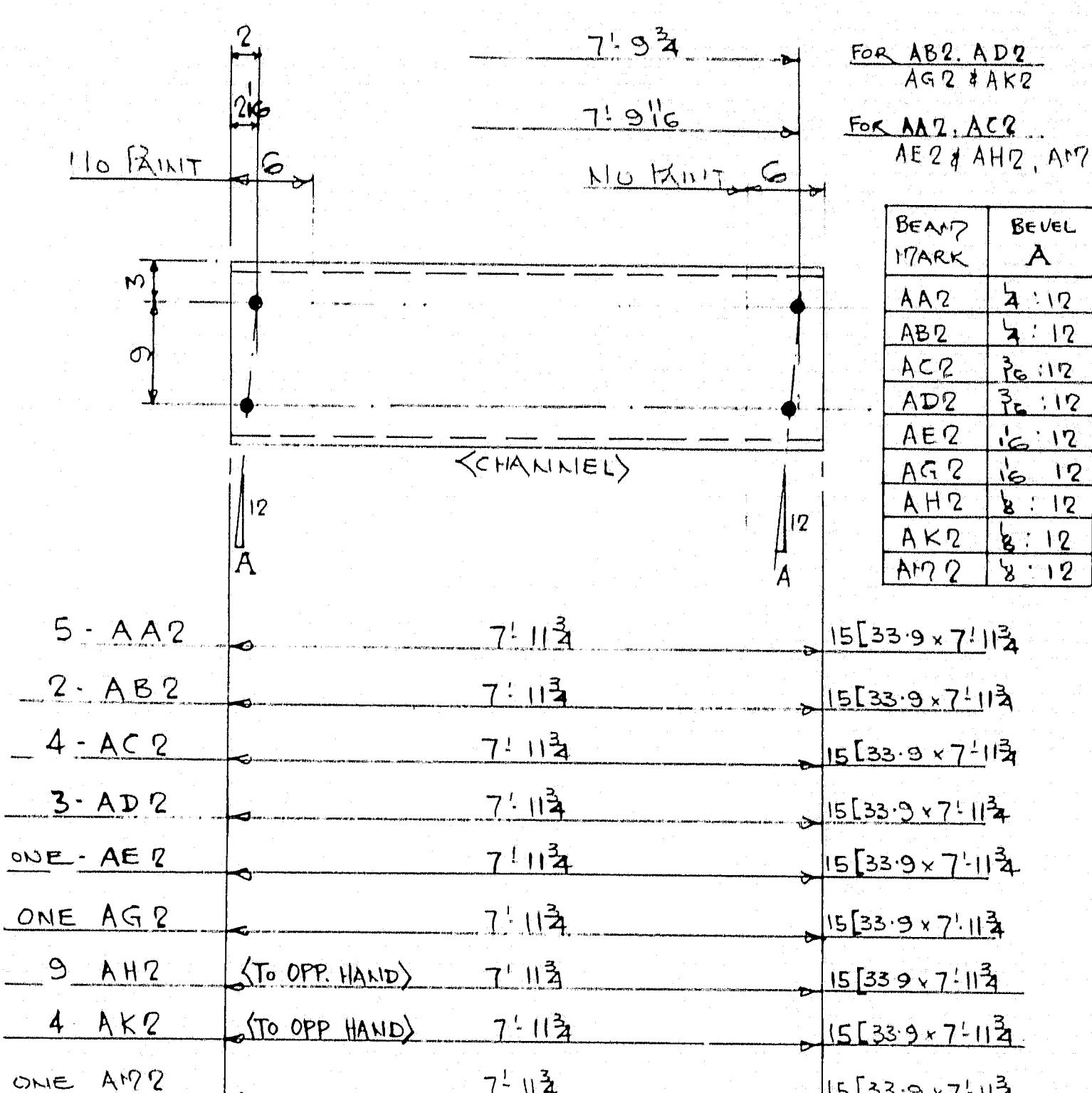
12-SPLICE RTS THUS 1X P H2
12-SPLICE RTS THUS 1X P K2

24-SPLICE RTS THUS 1X P I2
24-SPLICE RTS THUS 1X P N2

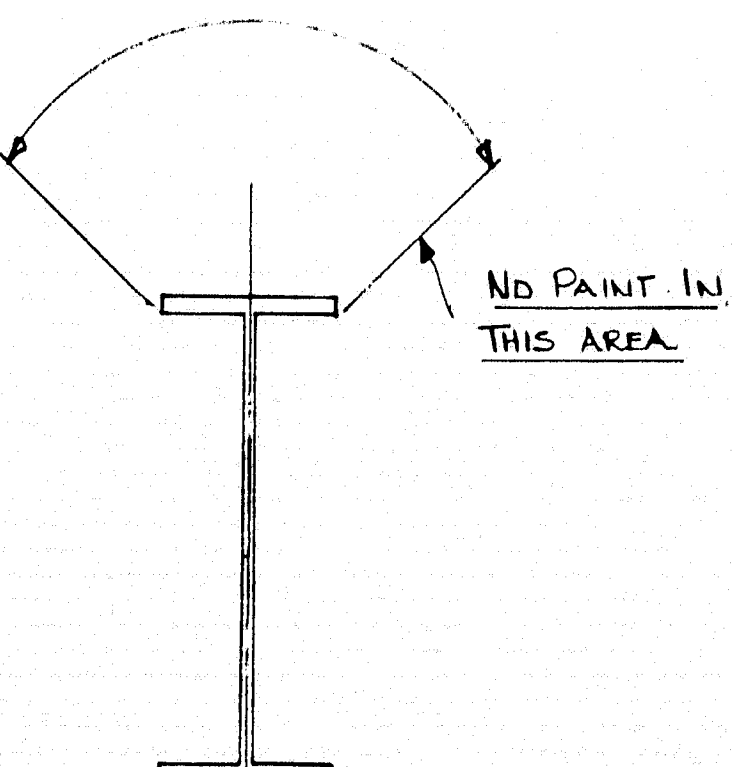


BEAM	BEVEL
P2	4:12
S2	4:12
T2	8:12
W2	4:12
X2	6:12
Y2	8:12

8-P2	7'-11 3/4"	16WF36x7'-11 3/4"
4-S2	7'-11 3/4"	16WF36x7'-11 3/4"
10-T2	7'-11 3/4"	16WF36x7'-11 3/4"
6-W2	7'-11 3/4"	16WF36x7'-11 3/4"
2-X2	7'-11 3/4"	16WF36x7'-11 3/4"
10-Y2	7'-11 3/4"	16WF36x7'-11 3/4"



5-AA2	7'-11 3/4"	15[33x9x7'-11 3/4"
2-AB2	7'-11 3/4"	15[33x9x7'-11 3/4"
4-AC2	7'-11 3/4"	15[33x9x7'-11 3/4"
3-AD2	7'-11 3/4"	15[33x9x7'-11 3/4"
ONE-AE2	7'-11 3/4"	15[33x9x7'-11 3/4"
ONE-AG2	7'-11 3/4"	15[33x9x7'-11 3/4"
9-AH2	(TO OPP. HAND) 7'-11 3/4"	15[33x9x7'-11 3/4"
4-AK2	(TO OPP. HAND) 7'-11 3/4"	15[33x9x7'-11 3/4"
ONE-AM2	7'-11 3/4"	15[33x9x7'-11 3/4"



SECTION A-A

PAINT
STD RED PRIMER
EXCEPT WHERE NOTED

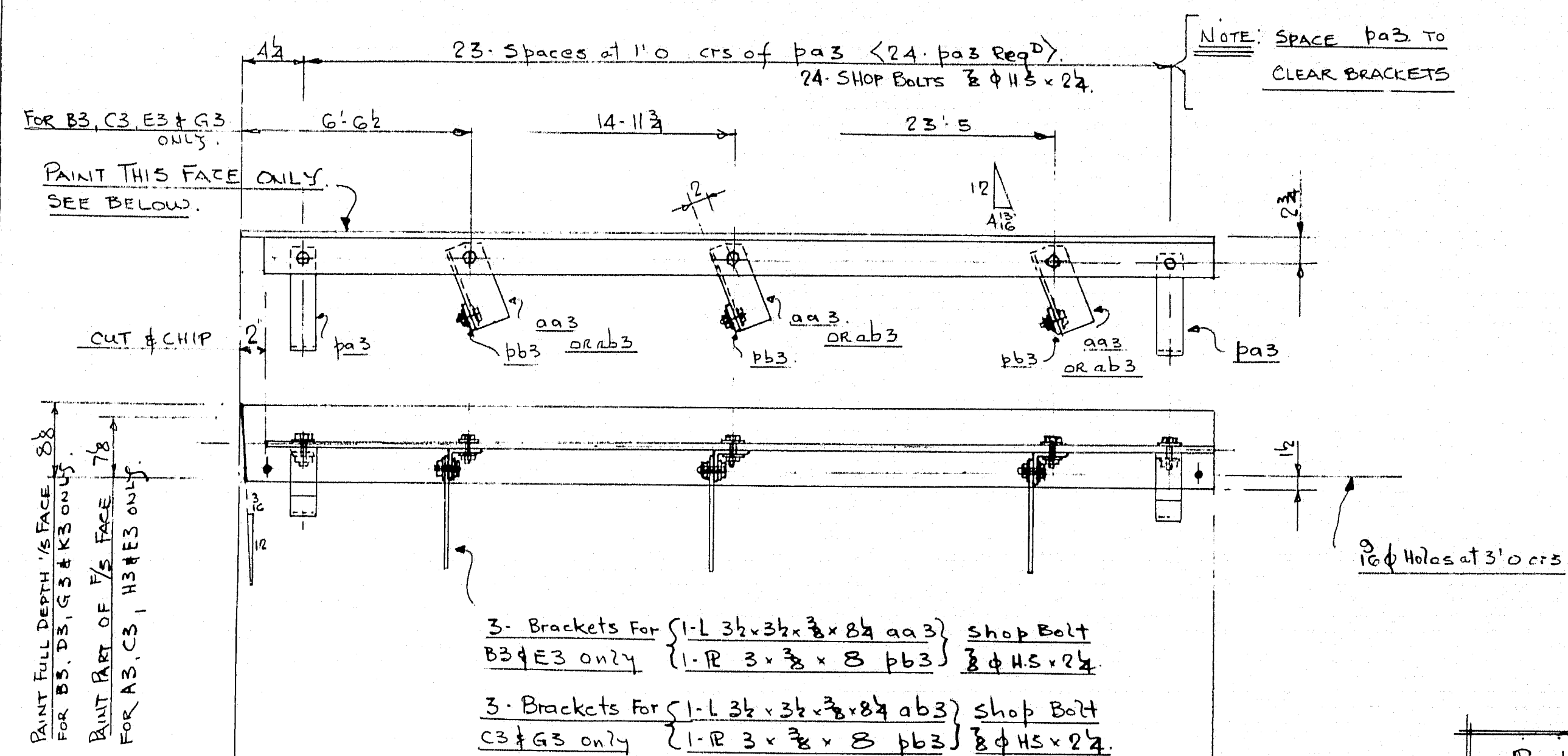
ALL HOLES 1 1/8 DIA.
BOLTS IN STRINGER SPICES 3/4 HS
BOLTS IN DIAPHRAGMS 3/4 COM 1/2

REVISIONS	DESCRIPTION	STRINGER + SPLICE PLT DETAILS
A	JOB	BRIDGE OVER NORTH MAIN STREET
B	PITTSFIELD MAINE	
C	ARCHT. STATE OF MAINE BRIDGE DEPT.	
D	CUSTOMER	REED + REED BATH - MAINE
E	AUGUSTA IRON WORKS	
F	AUGUSTA, MAINE	
G	ORDER	968
H		
I		
J		

86-200C

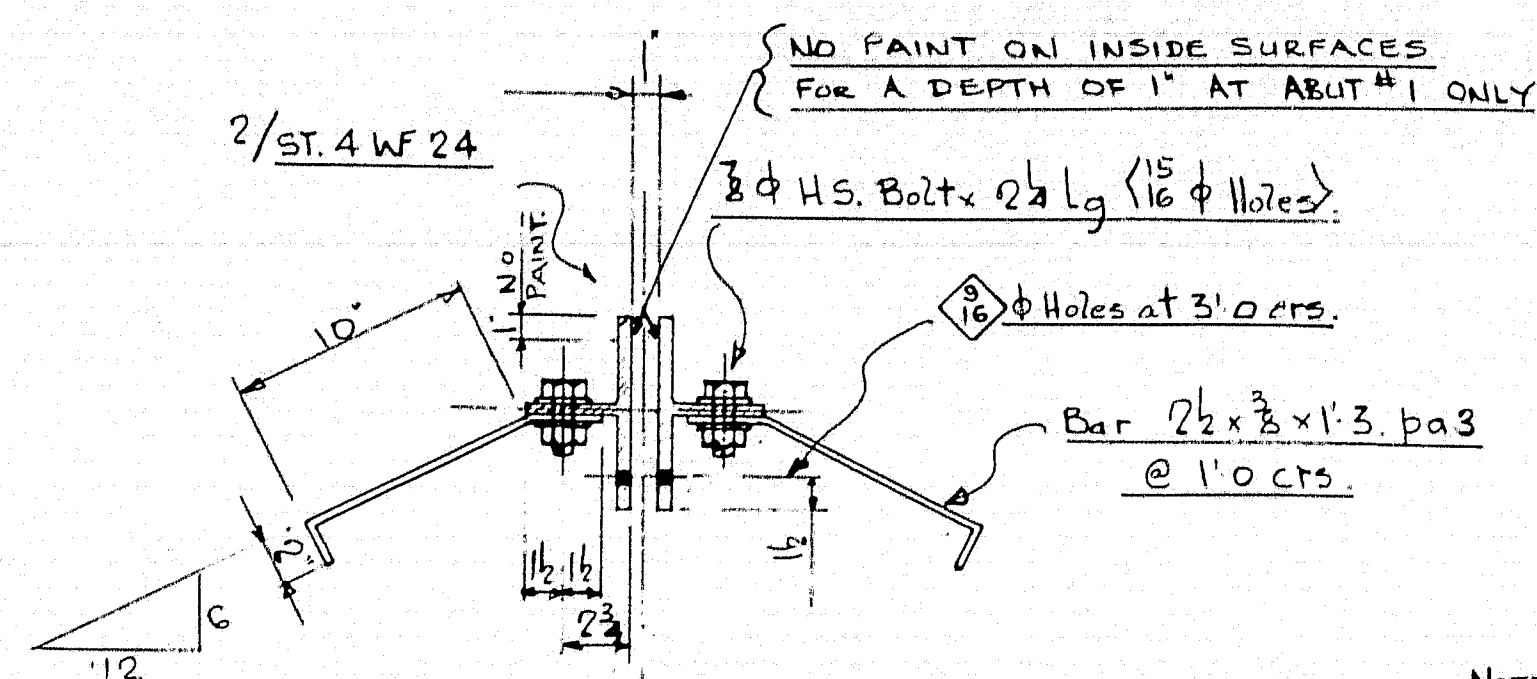
[illegible]

LAYOUT OF ARMORED JOINTS.



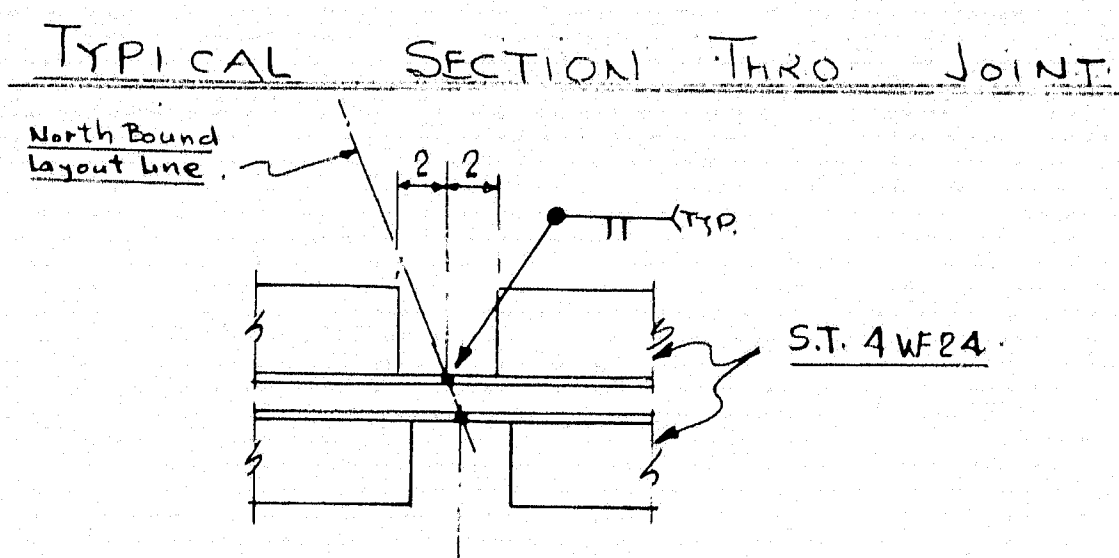
ONE A3	23' 8 1/2"	ST 4WF 24 x 23' 8 1/2"
ONE B3	23' 8 1/2"	ST 4WF 24 x 23' 8 1/2"
ONE C3	(TO OPP HAND)	ST 4WF 24 x 23' 8 1/2"
ONE D3	(TO OPP HAND)	ST 4WF 24 x 23' 8 1/2"
ONE E3	23' 8 1/2"	ST 4WF 24 x 23' 8 1/2"
ONE G3	(TO OPP HAND)	ST 4WF 24 x 23' 8 1/2"
ONE H3	(TO OPP HAND)	ST 4WF 24 x 23' 8 1/2"
ONE K3	23' 8 1/2"	ST 4WF 24 x 23' 8 1/2"

<u>SHOP NOTE</u>				
K3 Is The Same as A3 Except for Painting				
E3	"	"	B3	"
G3	"	"	C3	"
H3	"	"	D3	"

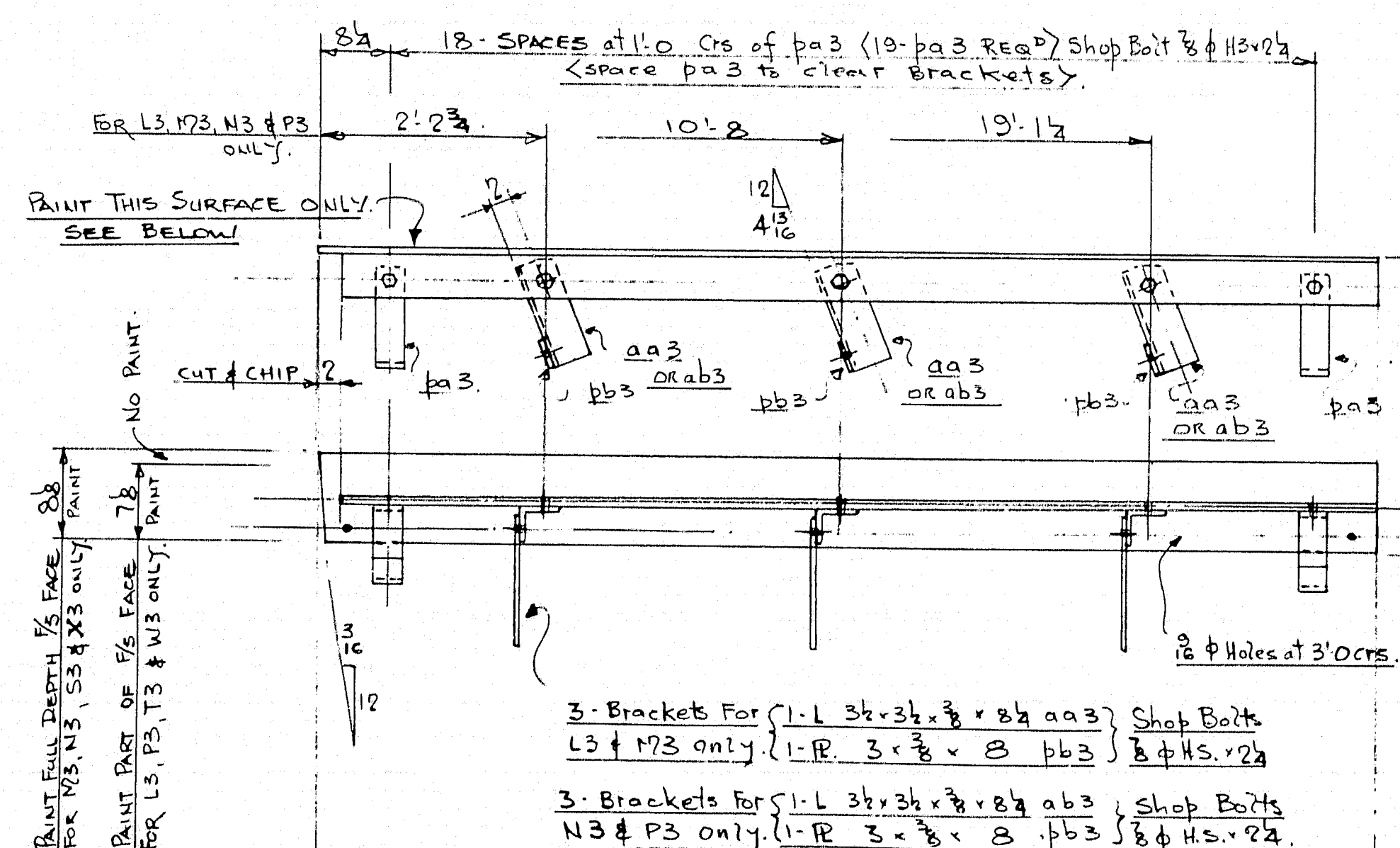


NOTE: PAINT THESE
FACES. ONLY
UNLESS NOTED

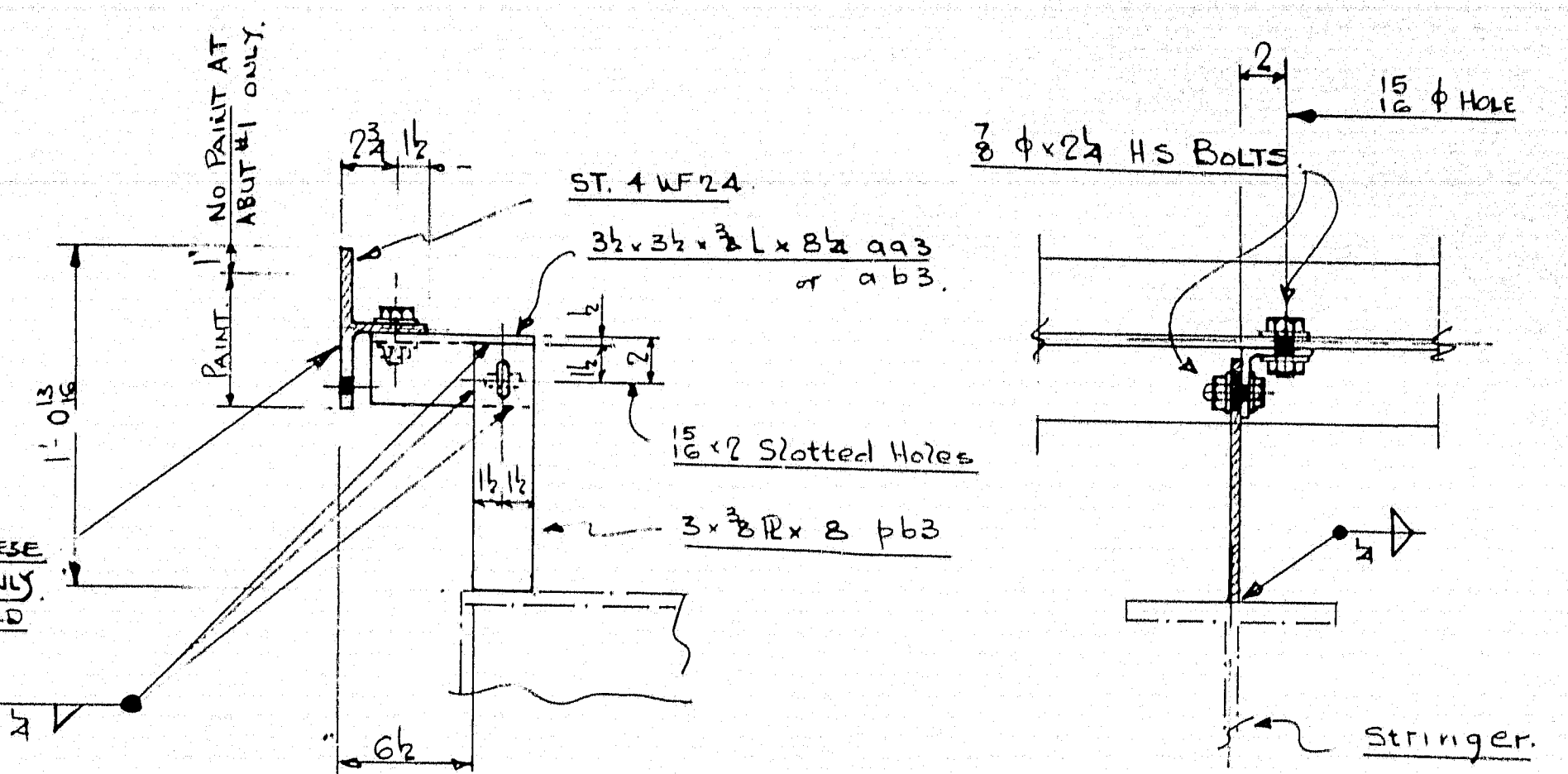
Field Weld Plate to
L. After Joint has
been Set In position



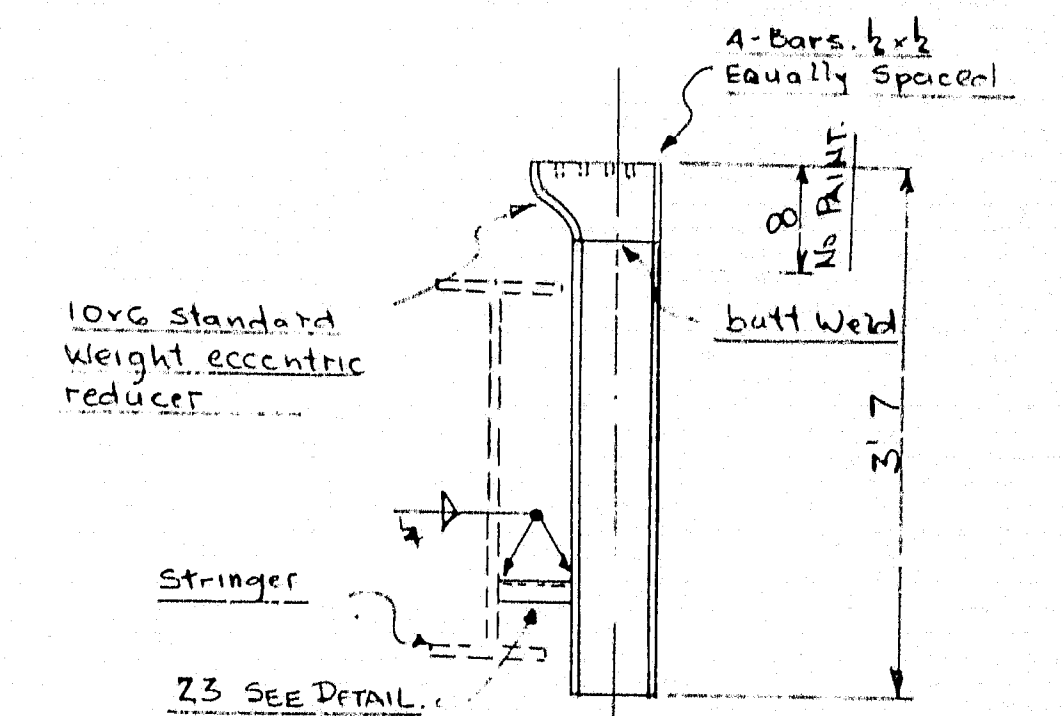
DETAIL A



ONE L3	19' 4 1/2	ST 4WF24 x 19' 4 1/2
ONE P3	19' 4 1/2	ST 4WF24 x 19' 4 1/2
ONE N3	(TO OPP HAND)	ST 4WF24 x 19' 4 1/2
ONE P3	(TO OPP HAND)	ST 4WF24 x 19' 4 1/2
ONE S3	19' 4 1/2	ST 4WF24 x 19' 4 1/2
ONE T3	19' 4 1/2	ST 4WF24 x 19' 4 1/2
ONE W3	(TO OPP HAND)	ST 4WF24 x 19' 4 1/2
ONE X3	(TO OPP HAND)	ST 4WF24 x 19' 4 1/2

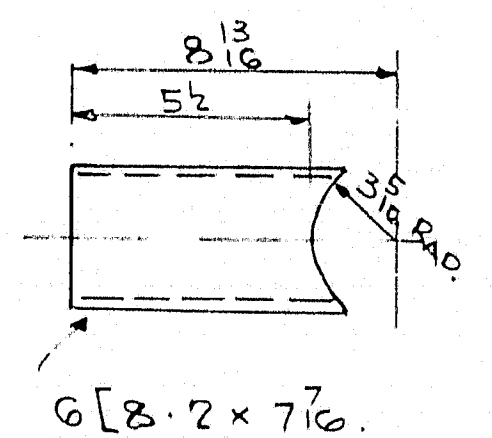


DETAIL OF JOINT ADJUSTMENT.



6" DIA Std. Pipe x 3'7" ALL.

12 - DRAINS THUS NR Y3.



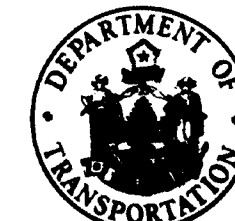
12. CHANNELS THIS MK Z3.

PAINT STD. RED PRIMER EXCEPT
WHERE NOTED
1/8" ϕ HOLES FOR 3/8" ϕ H.S. BOLTS UNLESS NOTED

REVISIONS	DESCRIPTION: ARMORED JOINT DETAILS	
A	JOB.	DRAWN BY DATE
B	BRIDGE OVER NORTH MAIN STREET	J.M.I.
C	PITTSFIELD MAINE	CHECKED BY DATE
D	ARCHT. STATE OF MAINE BRIDGE DEPT.	J.K.
E	CUSTOMER.	SHEET
F	REED & REED	3
G	BATH - MAINE	
H	AUGUSTA IRON WORKS	ORDER
J	AUGUSTA, MAINE	968

SHEET NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	IR-95-7(96)	1	14

STATE OF MAINE DEPARTMENT OF TRANSPORTATION



PLANS

BRIDGE WEARING SURFACE REPLACEMENTS

PROJECT NO. IR-95-7 (96)

I-95 NB over SEBASTICOOK RIVER (5990)
I-95 SB over SEBASTICOOK RIVER (1446)
I-95 NB over NORTH MAIN STREET (5989)
I-95 SB over NORTH MAIN STREET (1445)
I-95 NB over WEBB ROAD (5984)
I-95 SB over WEBB ROAD (1449)
I-95 NB over SOMERSET AVENUE (5985)
I-95 SB over SOMERSET AVENUE (1447)
I-95 NB over MAINE CENTRAL R.R. (5988)
I-95 NB over ROUTE 152 (5987)
I-95 SB over ROUTE 152 & MAINE CENTRAL R.R. (5986)

IN THE TOWN OF PITTSFIELD SOMERSET COUNTY

CONVENTIONAL SIGNS

COUNTY LINES	---	TRAVELLED WAY - PROPOSED	=====
TOWN LINES	---	UNDERGROUND UTILITIES - EXISTING	----
PROPERTY LINES	---	UNDERGROUND UTILITIES - PROPOSED	----
R/W LINES - EXISTING	---	RAILROAD - SINGLE TRACK	=====
R/W LINES - NEW - ACCESS CONTROL	---	RAILROAD - DOUBLE TRACK	=====
R/W LINES - NEW - NO ACCESS CONTROL	---	UTILITY POLE - EXISTING	----
CULVERT - EXISTING	---	UTILITY POLE - JOINT OCCUPANCY	----
CULVERT - PROPOSED	---	PROPOSED UTILITY POLE - TEMPORARY	----
CURBING - EXISTING	---	PROPOSED UTILITY POLE - PERMANENT	----
CURBING - PROPOSED	---	TREES	----
TRAVELLED WAY - EXISTING	=====	WOODS	----

INDEX OF SHEETS

SHEET DESCRIPTION

1. Title Sheet
2. Estimated Quantities
3. General Plans & Sections
4. General Plans
5. Sections
6. Joint Details
7. End Post & Guard Rail Connection Detail
8. Reinforcing Steel Schedule
9. BD 125-82 Expansion Device
10. BD 127-81 Miscellaneous Details (Barriers)
11. (HD-6) Type 3 Guard Rail (Terminal Connector)
- 12 thru 14. Traffic Control Plans

SPECIFICATIONS

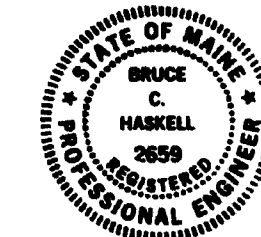
DESIGN: LOAD FACTOR DESIGN PER AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1983, AND INTERIM SPECIFICATIONS THROUGH 1985.

CONTRACT: STATE OF MAINE, DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS HIGHWAYS AND BRIDGES, REVISION OF JAN. 1984.

MATERIALS: CONCRETE--CLASS A-A
REINFORCING STEEL--ASTM A615, GRADE 60.

BASIC DESIGN STRESSES: CONCRETE-- $f'_c = 3,000$ PSI;
REINFORCING STEEL-- $f_y = 60,000$ PSI.

PLANS OF THE EXISTING BRIDGES ARE AVAILABLE FOR THE CONTRACTOR'S REFERENCE AT THE BRIDGE DESIGN OFFICE IN AUGUSTA. THE PLANS ARE REPRODUCTIONS OF ORIGINAL DRAWINGS AS PREPARED FOR THE CONSTRUCTION OF THE BRIDGES AND IT IS VERY UNLIKELY THAT THE PLANS WILL SHOW ANY CONSTRUCTION FIELD CHANGES OR ANY ALTERATIONS WHICH MAY HAVE BEEN MADE TO THE BRIDGES DURING THEIR LIFE SPAN.



CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

UNITED STATES
DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 1

APPROVED:

DIVISION ADMINISTRATOR DATE

APPROVED:

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
COMMISSIONER

DATE

June 13, 1989

Richard Coleman
CHIEF ENGINEER

June 13, 1989

99-459

NOTE

All work contemplated under this contract to be governed by and in conformity with the STANDARD SPECIFICATIONS (revision of January 1984) and supplementals thereto, except as modified on the plans and in the special provisions.

3538

BRUNING 44 132 6 323

JANUARY 1985

2504 2504

PROJECT DESIGN ENGINEER DATE	BY	
	DATE	
	DESIGN - DETAIL	
	CHECKED	
PLANS	REVISIONS	
	FIELD CHANGES	

ESTIMATED QUANTITIES		TOTAL	S.B. OVER N.B. OVER S.B. OVER N.B. OVER S.B. OVER N.B. OVER S.B. OVER N.B. OVER S.B. OVER N.B. OVER S.B. OVER N.B. OVER											
ITEM NO.	DESCRIPTION	QUANTITY	UNIT	SEBASTICOOK RIVER	SEBASTICOOK RIVER	N. MAIN STREET	N. MAIN STREET	WEBB ROAD	WEBB ROAD	SOMERSET AVENUE	SOMERSET AVENUE	M.C.R.R. ROUTE 152	M.C.R.R. ROUTE 152	N.B. OVER ROUTE 152
202.127	REMOVAL OF EXISTING BITUMINOUS PAVEMENT	1	L.S.	0.12	0.12	0.07	0.07	0.07	0.07	0.08	0.08	0.17	0.08	0.07
202.202	Removing Pavement Surface	7377	S.Y.	683	683	683	683	683	683	683	683	683	615	615
403.10	HOT BITUMINOUS PAVEMENT GRADING D	1898	TONS	210	210	146	146	146	146	151	151	286	155	151
403.121	HOT BITUMINOUS PAVEMENT GRADING E (SHIMMING)	108	TONS	10	10	10	10	10	10	10	10	10	9	9
410.15	Emlulified Asphalt Applied	736	G	68	68	68	68	68	68	68	68	68	62	62
503.12	Reinforcing Steel, Fabricated and Delivered	5448	Lbs.	503	503	503	503	503	503	503	503	418	503	503
503.13	Reinforcing Steel, Placing	5448	Lbs.	503	503	503	503	503	503	503	503	418	503	503
506.142	FIELD PAINT EXISTING STRUCTURAL STEEL	1	L.S.	0.15	0.15	0.06	0.06	0.06	0.06	0.07	0.07	0.17	0.08	0.07
508.13	MEMBRANE WATERPROOFING	1	L.S.	0.11	0.11	0.08	0.08	0.08	0.08	0.08	0.08	0.14	0.08	0.08
514.06	CURING BOX FOR CONCRETE CYLINDERS	1	EACH											
518.30	REHAB. OF STRUCTURAL CON. SLAB-TO-REINFORCING STEEL	5625	S.F.	445	445	217	163	162	162	229	229	1310	929	890
518.31	REHAB. OF STRUCTURAL CON. SLAB-TO-BELOW REINF. STEEL	2655	S.F.	267	445	108	0	0	0	114	114	393	619	595
520.2401	BRIDGE JOINT MODIFICATION	10	EA.	1	1	1	2	1	1	1	1	1	1	1
520.2402	BRIDGE JOINT MODIFICATION	5	EA.	1	1	1	1	1	1	1	1	1	1	1
520.2403	Bridge Joint Modification	6	EA.	1	1	1	1	1	1	1	1	1	1	1
526.301	Temporary Concrete Barrier, Type 1	1	L.S.	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.05	0.05
606.173	Bridge Connections	22	EA.	2	2	2	2	2	2	2	2	2	2	2
627.611	6 inch Solid White Pavement Marking Line	3,600	L.F.	360	360	360	360	360	360	360	360	360	180	180
627.621	6 inch Broken White Pavement Marking Line	3,600	L.F.	360	360	360	360	360	360	360	360	360	180	180
627.631	6 inch Solid Yellow Pavement Marking Line	3,600	L.F.	360	360	360	360	360	360	360	360	360	180	180
627.67	Removing Pavement Markings	1,600	S.F.	160	160	160	160	160	160	160	160	160	80	80
627.681	Temporary 6 inch Painted Pavement Marking Line, Yellow or White	3,600	L.F.	360	360	360	360	360	360	360	360	360	180	180
627.69	Temporary 4 inch Plastic Pavement Marking Line, Yellow or White	1,600	L.F.	160	160	160	160	160	160	160	160	160	80	80
638.20	Field Office Type C	1	EA.	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.05	0.05
652.30	Flashing Arrow Board	2	EA.	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.10	0.10
652.31	Type 1 Barricade	100	EA.	10	10	10	10	10	10	10	10	10	5	5
652.33	Drum	20	EA.	2	2	2	2	2	2	2	2	2	1	1
652.34	Cone	20	EA.	2	2	2	2	2	2	2	2	2	1	1
652.35	Construction Signs	700	S.F.	70	70	70	70	70	70	70	70	70	35	35
652.361	Maintenance of Traffic Control Devices	1	L.S.	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.05	0.05
652.38	Flagger	500	M.H.	50	50	50	50	50	50	50	50	50	25	25
659.10	Mobilization	1	L.S.	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.10	0.05	0.05
= BREAKDOWN OF LUMP SUM QUANTITIES =														
202.127	REMOVAL OF EXISTING BITUMINOUS PAVEMENT	8420	S.Y.	987	987	599	599	598	598	631	631	1442	683	660
506.142	FIELD PAINT EXISTING STRUCTURAL STEEL	1234500	LB.	287800	287800	115350	115350	113000	113000	132500	132500	355600	147800	133800
508.13	MEMBRANE WATERPROOFING	8420	S.Y.	987	987	599	599	598	598	631	631	1442	683	660

F.R.A.	STATE	PROJECT NUMBER	SHEET	TOTAL
1	MAINE	IR-95-7(96)	2	14

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

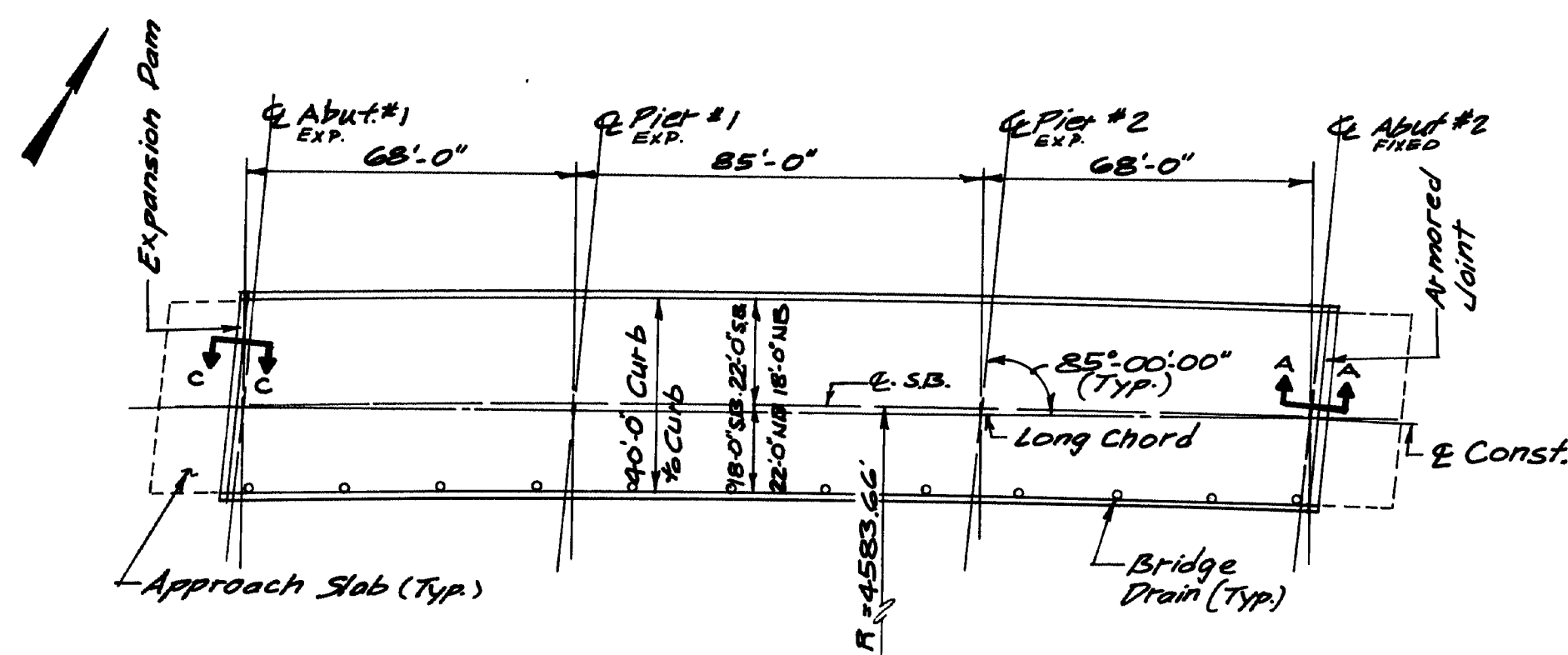
STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PITTSFIELD
ESTIMATED QUANTITIES

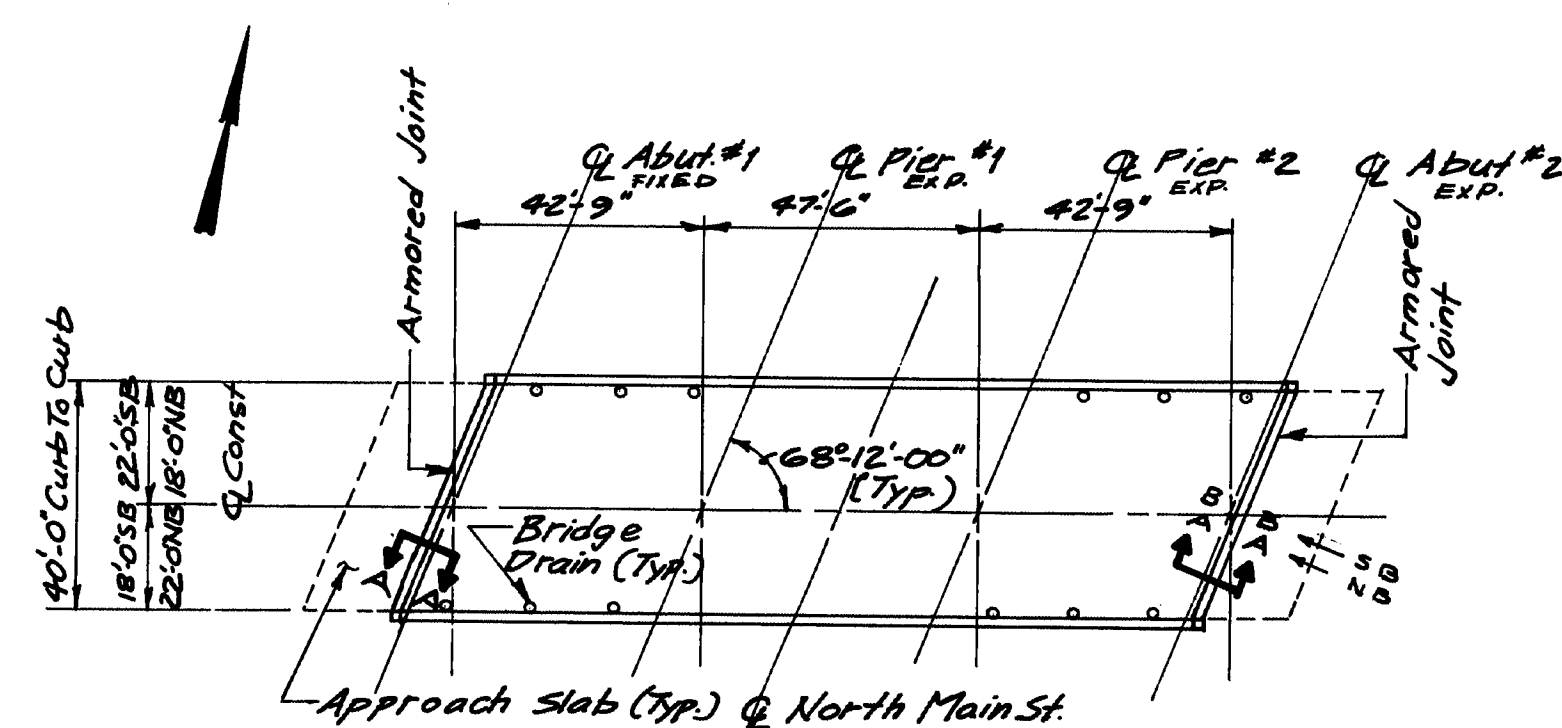
99-460

A. b. + Dec 1999
Kpm

DATE	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	IR-95-7(14)	3	14



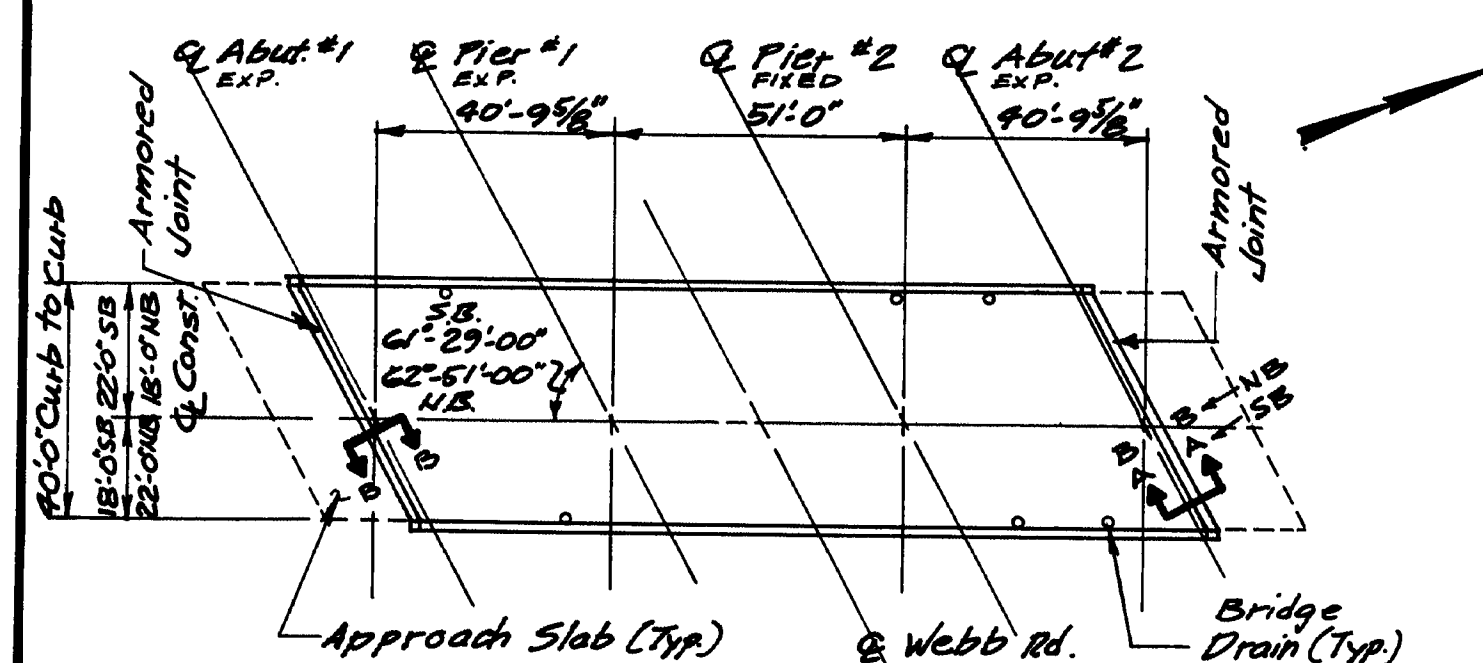
I-95 SOUTHBOUND AND NORTHBOUND OVER SEBASTICOOK RIVER 5990 N.B. 1446 S.B.



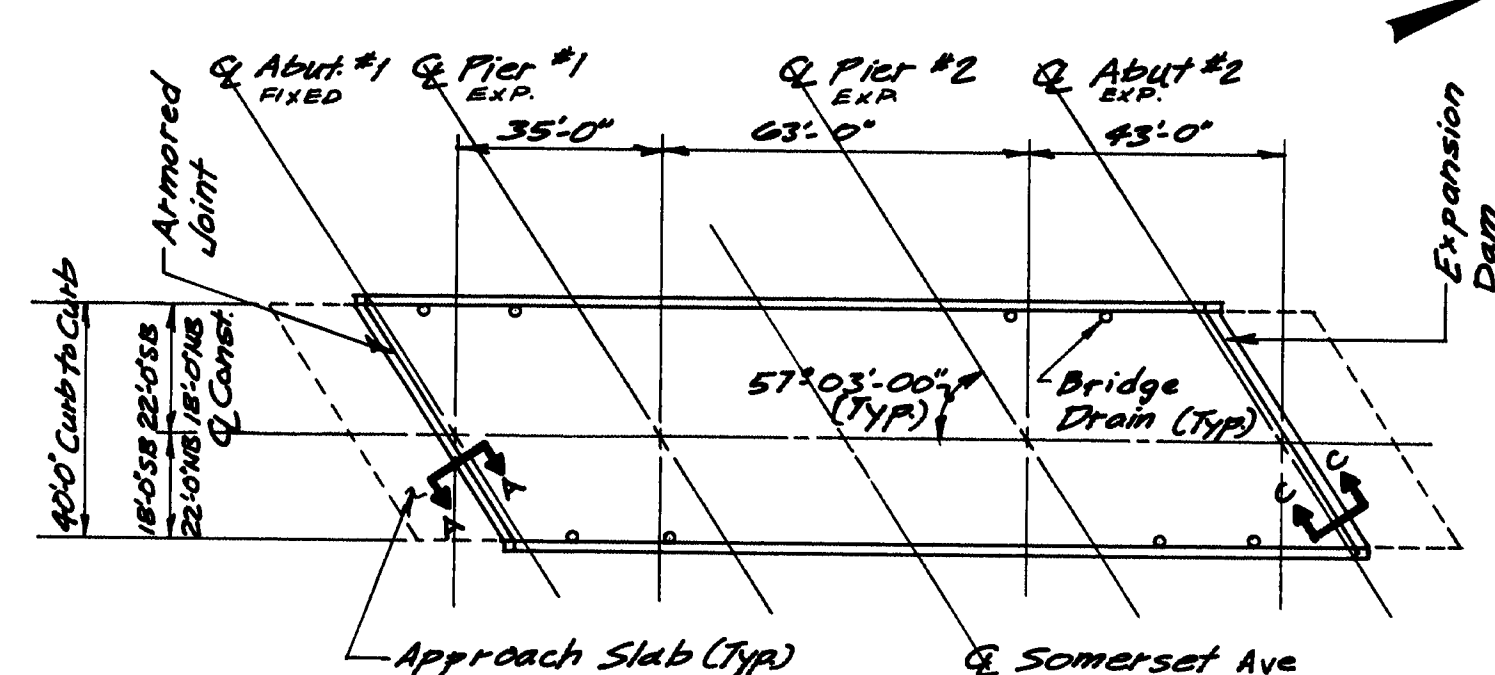
I-95 SOUTHBOUND AND NORTHBOUND OVER NORTH MAIN STREET 5989 N.B. 1445 S.B.

SCOPE OF WORK

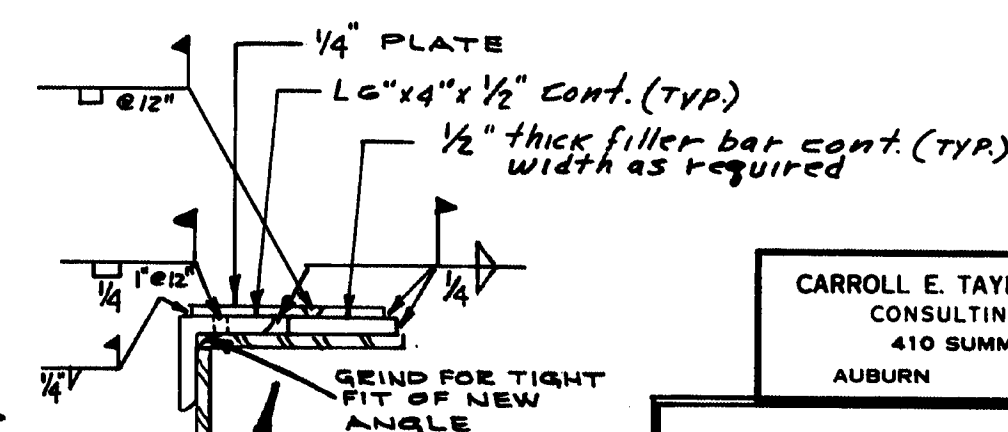
- ALL BRIDGES:
 REMOVE 2-INCH BITUMINOUS WEARING SURFACE AND REMOVE EXISTING MEMBRANE WATER-PROOFING.
 REPAIR DECK AS NECESSARY.
 MODIFY EXPANSION JOINTS *where indicated*.
 INSTALL 3-INCH BITUMINOUS WEARING SURFACE PLUS MEMBRANE *waterproofing*.
 CLEAN AND PAINT ALL STRUCTURAL STEEL.
 MODIFY APPROACH PAVEMENT FOR NEW GRADES.



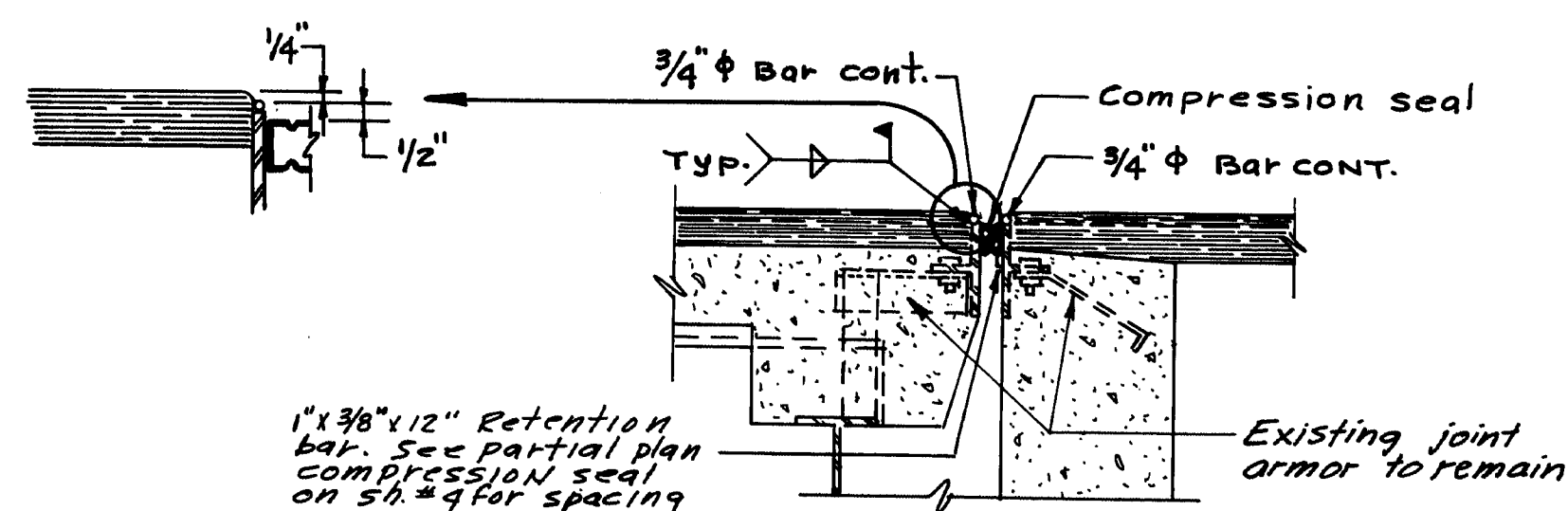
I-95 SOUTHBOUND AND NORTHBOUND OVER WEBB ROAD 5984 N.B. 1449 S.B.



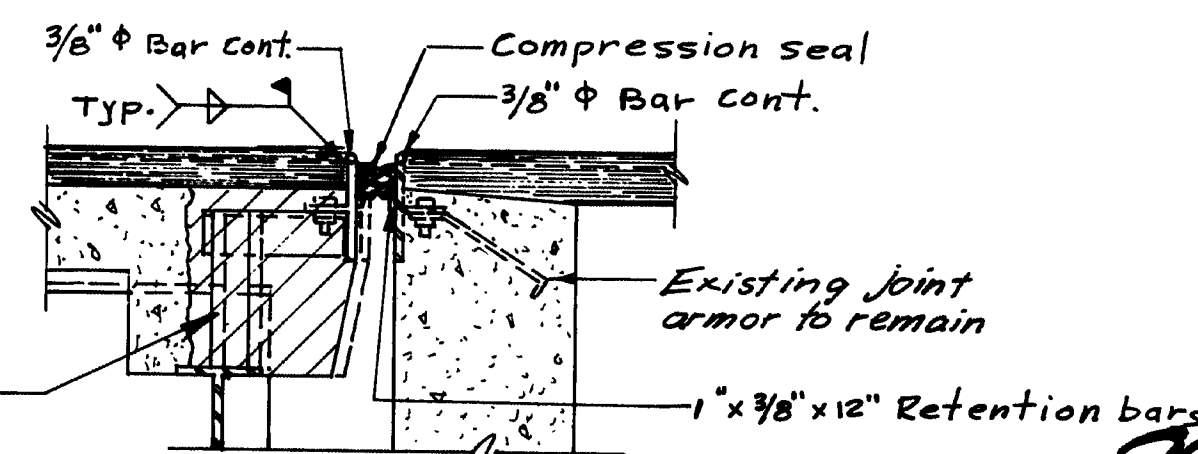
I-95 SOUTHBOUND AND NORTHBOUND OVER SOMERSET AVENUE 5985 N.B. 1447 S.B.



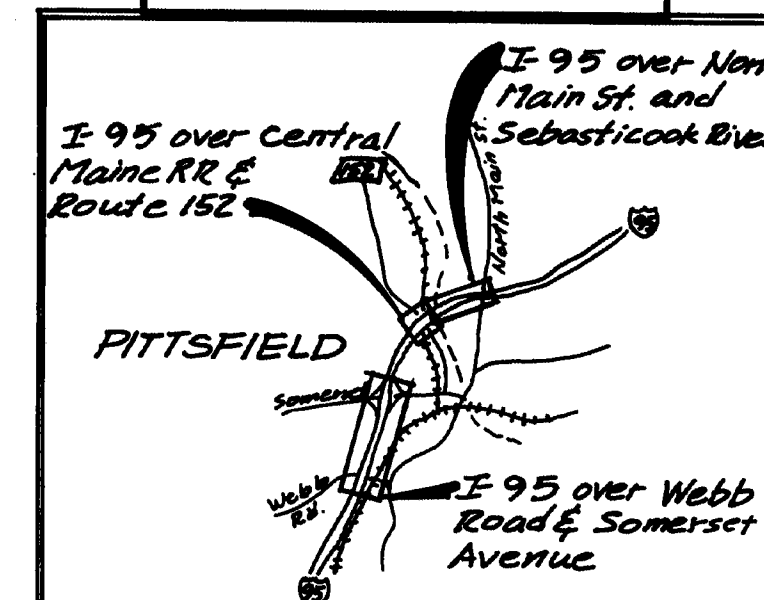
SECTION C-C



SECTION A-A



SECTION B-B



LOCATION MAP

0 1 2 3 4
Scale in miles

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95
SOUTHBOUND AND NORTHBOUND
OVER

Sebasticook River, North Main
Street, Webb Road, Somerset
Avenue, Central Maine RR & Route 152
PITTSFIELD

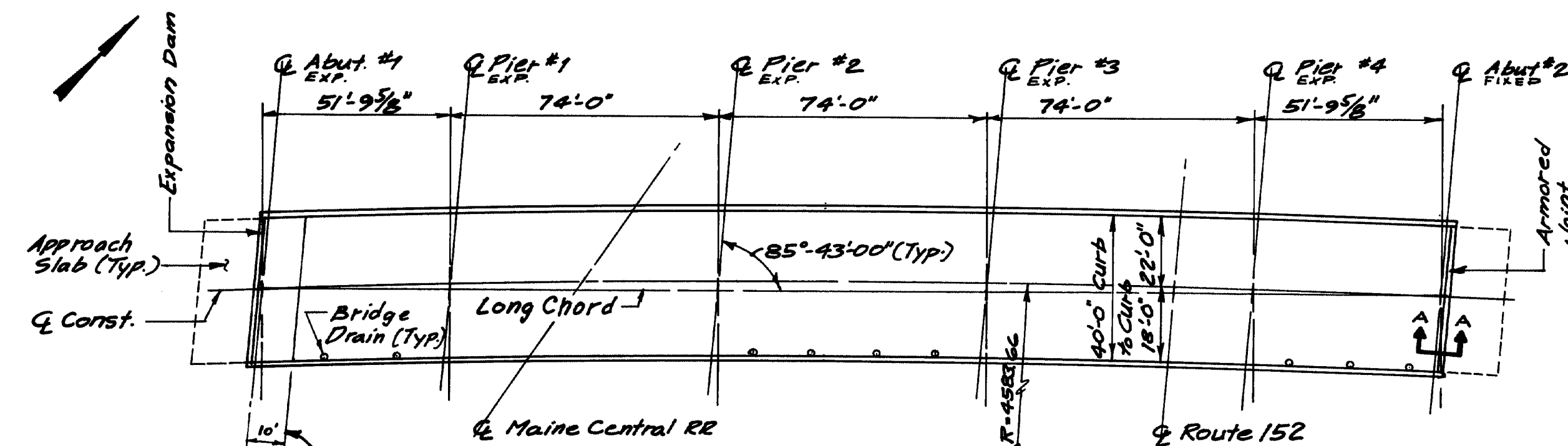
GENERAL PLAN & SECTIONS

SHEET 3 OF 14 AUGUSTA, MAINE

PROJECT ENGINEER	BY	DATE
DESIGN - DETAIL	DAY	2-2-11
CHECKED	B.E.H.	
REVISIONS		
FIELD CHANGES		

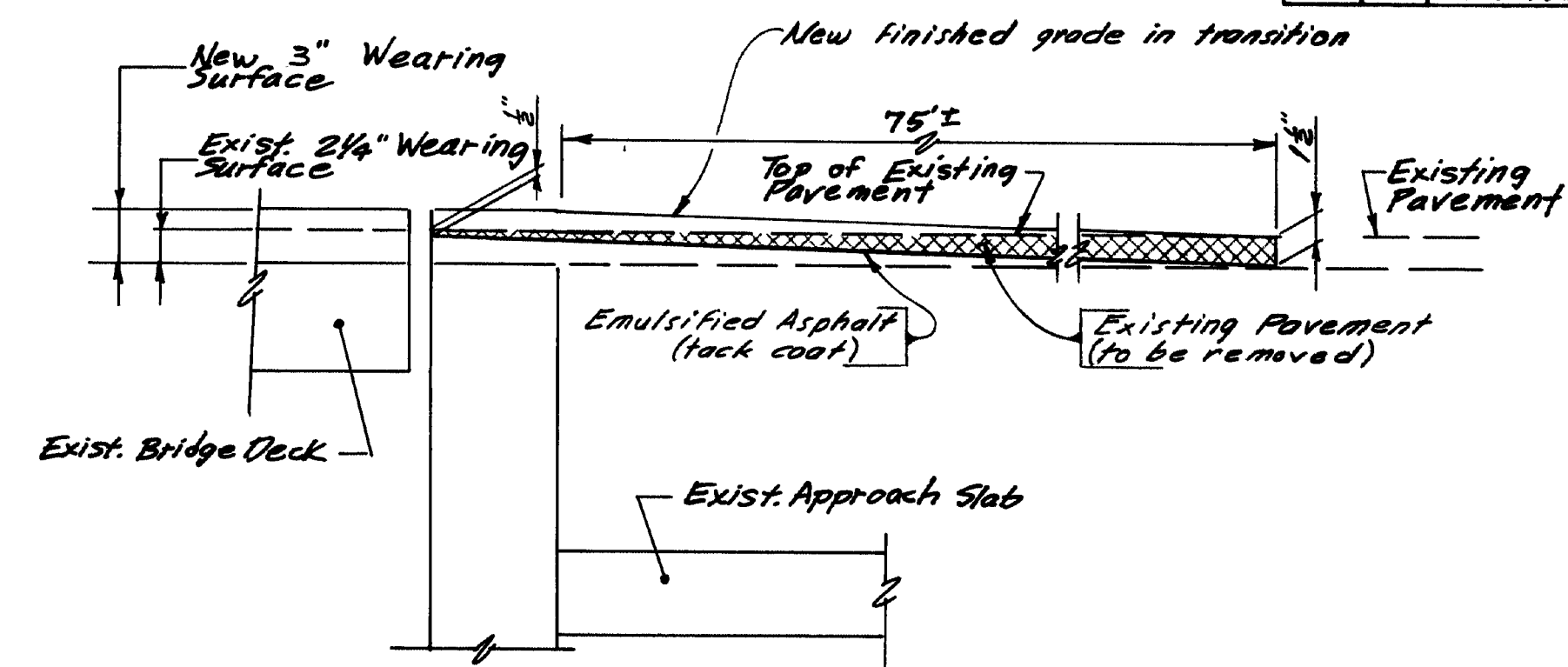
99-461
As bu 14 Dec 1999

F.R. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	IR-95-7(96)	4	14

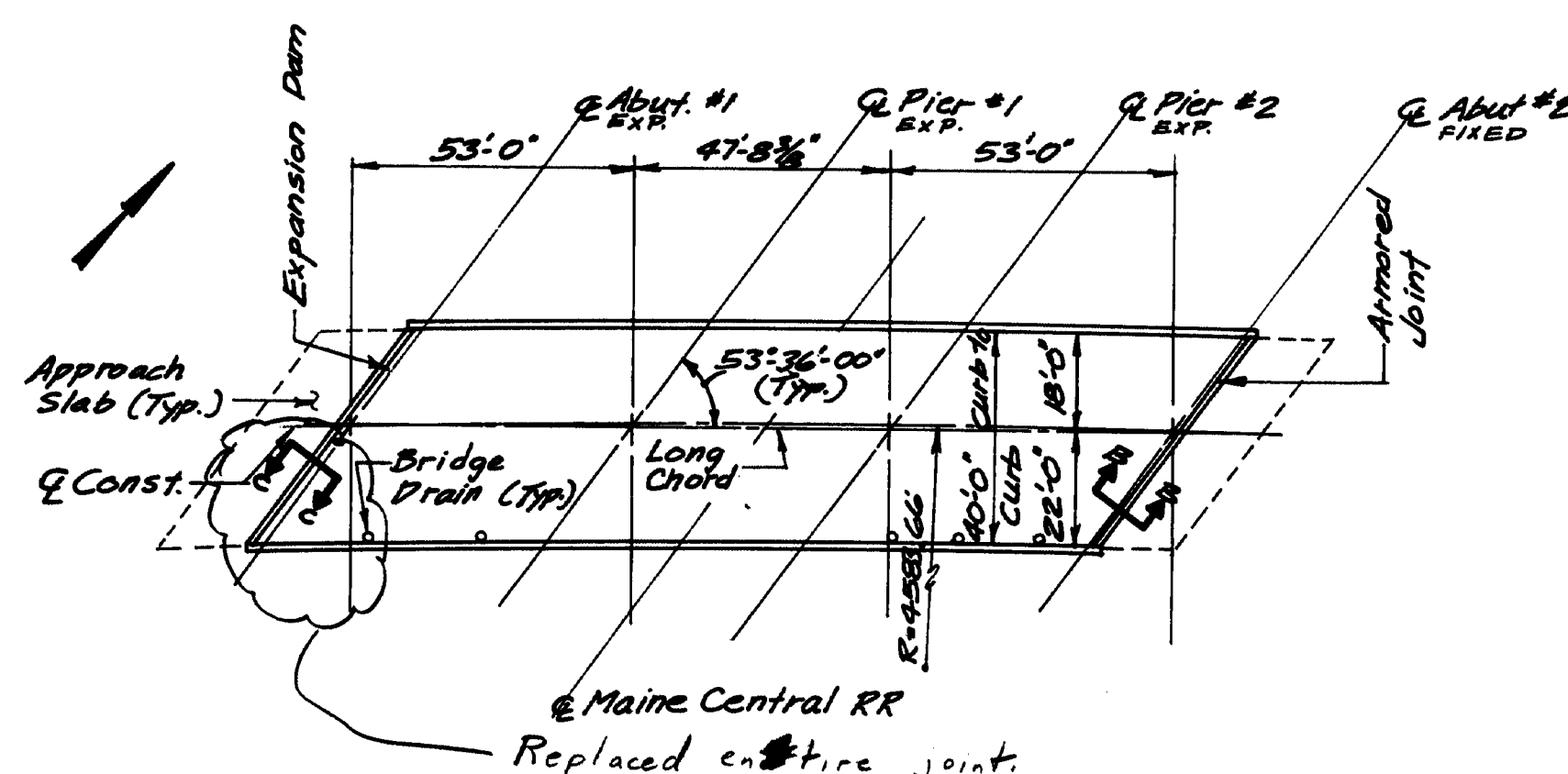


TRANSITION NEW PAVEMENT THICKNESS TO MATCH EXIST. FINGER JOINT HEIGHT

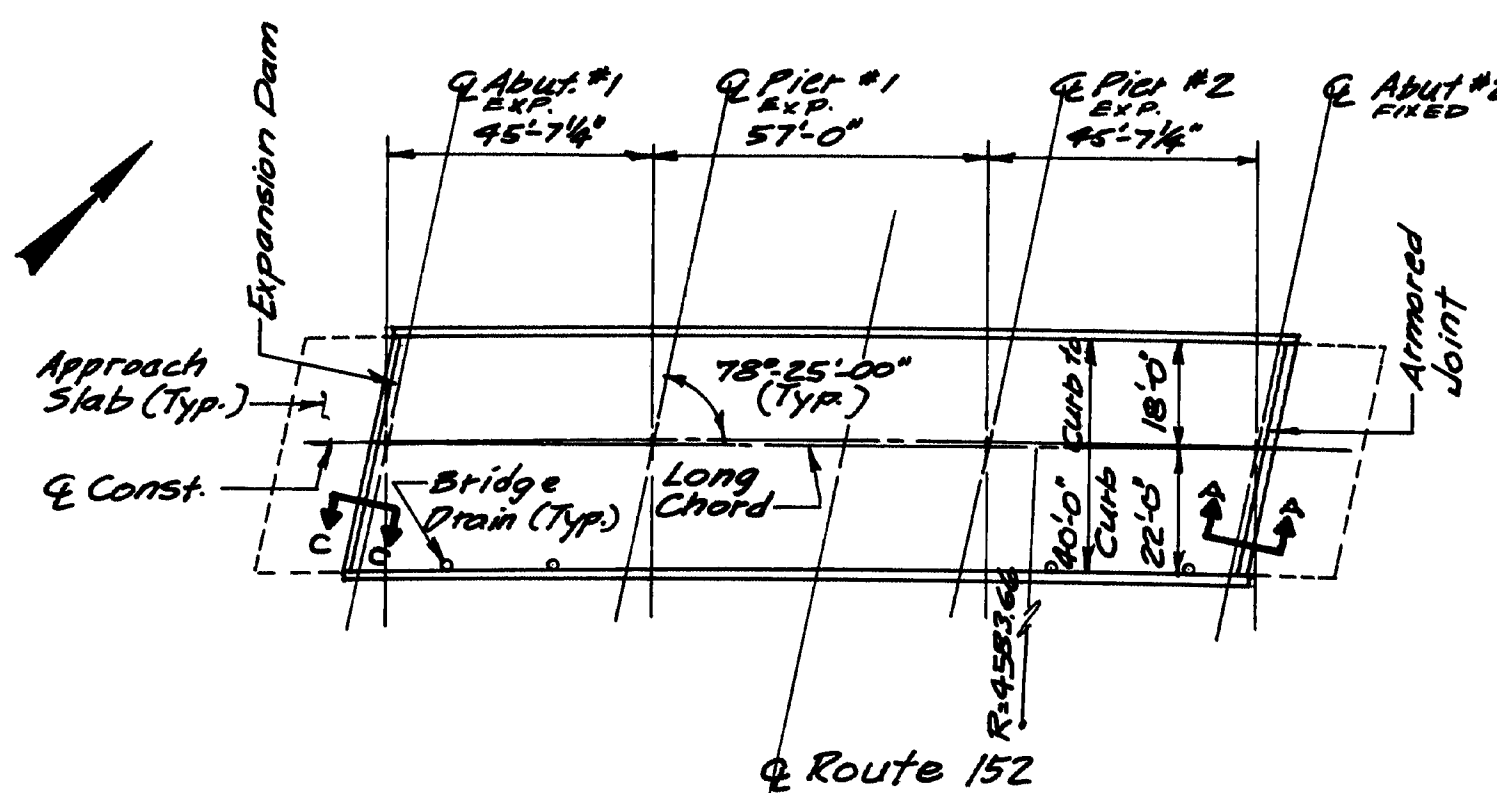
I-95 SOUTHBOUND OVER MAINE CENTRAL RR AND ROUTE 152 5986



APPROACH PAVEMENT TRANSITION
(Typical all bridges, Each end.)



I-95 NORTHBOUND OVER MAINE CENTRAL RR 5988



I-95 NORTHBOUND OVER ROUTE 152 5987

CONSTRUCTION NOTES

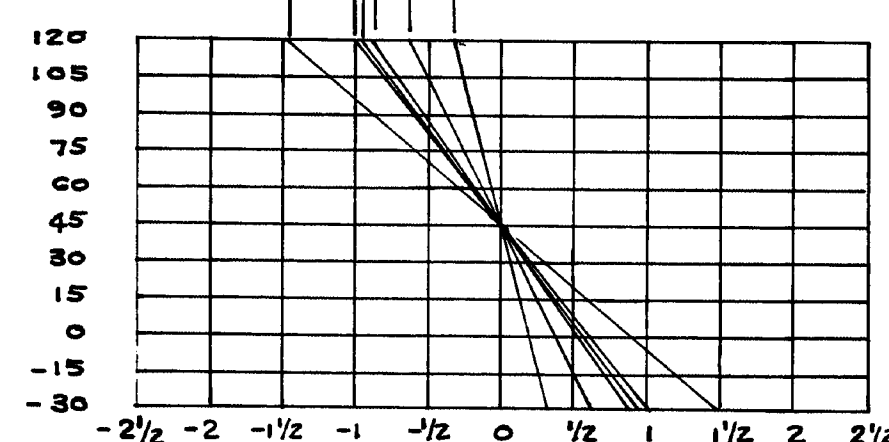
1. MAINTAIN ONE 1/3-FOOT MINIMUM TRAFFIC LANE AT ALL TIMES.
2. ALL WORK SHALL BE DONE BEHIND TEMPORARY CONCRETE BARRIERS.
3. THE TOP SURFACE OF THE EXISTING CONCRETE SLABS SHALL BE REPAIRED AS DIRECTED BY THE ENGINEER.
4. DEPRESS THE BITUMINOUS WEARING SURFACE AROUND THE EXISTING BRIDGE DRAINS AS DIRECTED BY THE ENGINEER.
5. BECAUSE OF STAGED CONSTRUCTION, SOME EXPANSION JOINTS MAY REQUIRE CONSTRUCTION JOINTS. THESE SHALL BE AS APPROVED BY THE ENGINEER AS TO TYPE AND LOCATIONS.
6. PAYMENT FOR DRILLING AND GROUTING ASSOCIATED WITH MODIFICATIONS OF THE JOINTS WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
7. PAYMENT FOR REMOVING AND RE-INSTALLING BRIDGE RAIL OR GUARDRAIL, AS NEEDED TO ACCOMPLISH JOINT MODIFICATIONS, WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
8. PAYMENT FOR REMOVING AND REPLACING PORTIONS OF END POSTS (CONCRETE PARAPET AND CURB) WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
9. PAYMENT FOR CUTTING, REMOVING, OR REPLACING GRANITE CURB WHERE CALLED FOR WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
10. REINFORCING STEEL SHALL HAVE A MINIMUM COVER OF 2 INCHES UNLESS NOTED OTHERWISE.
11. WHERE GRANITE CURB IS CALLED FOR TO BE REMOVED TO MODIFY A JOINT OR TO INSTALL A SEAL, ONLY THE SMALLEST AMOUNT NECESSARY TO ACCOMPLISH THE WORK SHALL BE REMOVED. IT IS PREFERABLE TO REMOVE CURB TO AN EXISTING JOINT BUT IF A JOINT IS MORE THAN 3 FEET AWAY THE CURB SHALL BE SAW CUT IN THE FIELD. THE DECISION ABOUT WHETHER TO CUT OR NOT, AND WHERE, SHALL BE THE ENGINEER'S.
12. SEE STANDARD DETAIL SH 80125-02 FOR ADDITIONAL EXPANSION DEVICE DETAILS.
13. THE REMOVAL OF THE EXISTING BITUMINOUS PAVEMENT ON THE APPROACHES SHALL be paid for under Item 202.202.

1. SEALS TO BE FURNISHED SHALL HAVE A MOVEMENT RATING OF:

Exist. - NB & SB N. Main St. (Abut. #1)
2 7/8" - SB/SEBASTICOOK R. (ABUT. #1)
2 7/8" - NB/SEBASTICOOK R. (ABUT. #1)
2" - SB/MCRR (ABUT. #1)
1 7/8" - SB/SOMERSET AVE. (ABUT. #2)
1 7/8" - NB/SOMERSET AVE. (ABUT. #2)
1 7/8" - NB/ROUTE 152 (ABUT. #1)
1 3/4" - SB/N. MAIN ST. (ABUT. #2)
1 3/4" - NB/N. MAIN ST. (ABUT. #2)
1 1/4" - SB/WEBB ROAD (ABUT. #1)
1 1/4" - NB/WEBB ROAD (ABUT. #1)
5/8" - SB/WEBB ROAD (ABUT. #2)
5/8" - NB/WEBB ROAD (ABUT. #2)
EXIST. - SB/SOMERSET AVE. (ABUT. #1)
EXIST. - SB/SEBASTICOOK R. (ABUT. #2)
EXIST. - NB/SEBASTICOOK R. (ABUT. #2)
EXIST. - NB/SOMERSET AVE. (ABUT. #1)
EXIST. - SB/MCRR & ROUTE 152 (ABUT. #2)
5/8" - NB/MCRR (ABUT. #2)
EXIST. - NB/ROUTE 152 (ABUT. #2)

2. SEALS SHALL BE APPROVED BY THE ENGINEER PRIOR TO FABRICATION OF THE JOINT ARMOR.
3. COMPRESSION SEAL JOINT OPENINGS WILL VARY DEPENDING ON THE DIMENSIONS OF THE SEAL SELECTED BY THE CONTRACTOR. THE JOINT OPENING SHALL BE SET ACCORDING TO THE OPENING SHOWN ON THE APPROVED SHOP DETAIL DRAWINGS.
4. THE SEAL ADJUSTMENT CHART SHOWS THE ADJUSTMENT NECESSARY TO ADJUST THE JOINT OPENING SHOWN ON THE SHOP DETAIL DRAWINGS FOR TEMPERATURES OTHER THAN 45°F. ADJUSTMENT IS TO BE MEASURED PARALLEL TO THE CENTERLINE OF CONSTRUCTION.

I-95 N.B. & S.B. OVER SOMERSET AVE. - ABUT. 2
I-95 N.B. OVER ROUTE 152 - ABUT. 1
I-95 N.B. OVER MCRR - ABUT. 1
I-95 N.B. & S.B. OVER SEBASTICOOK RIVER
I-95 S.B. & N.B. OVER N. MAIN STREET
I-95 S.B. & N.B. OVER WEBB ROAD - ABUT. #1
I-95 S.B. & N.B. OVER WEBB ROAD - ABUT. #2



SETTING

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95
SOUTHBOUND AND NORTHBOUND
OVER

Sebasticook River, North Main
Street, Webb Road, Somerset
Avenue, Central Maine RR & Route 152
PITTSFIELD

GENERAL PLANS

SHEET 4 OF 14 AUGUSTA, MAINE

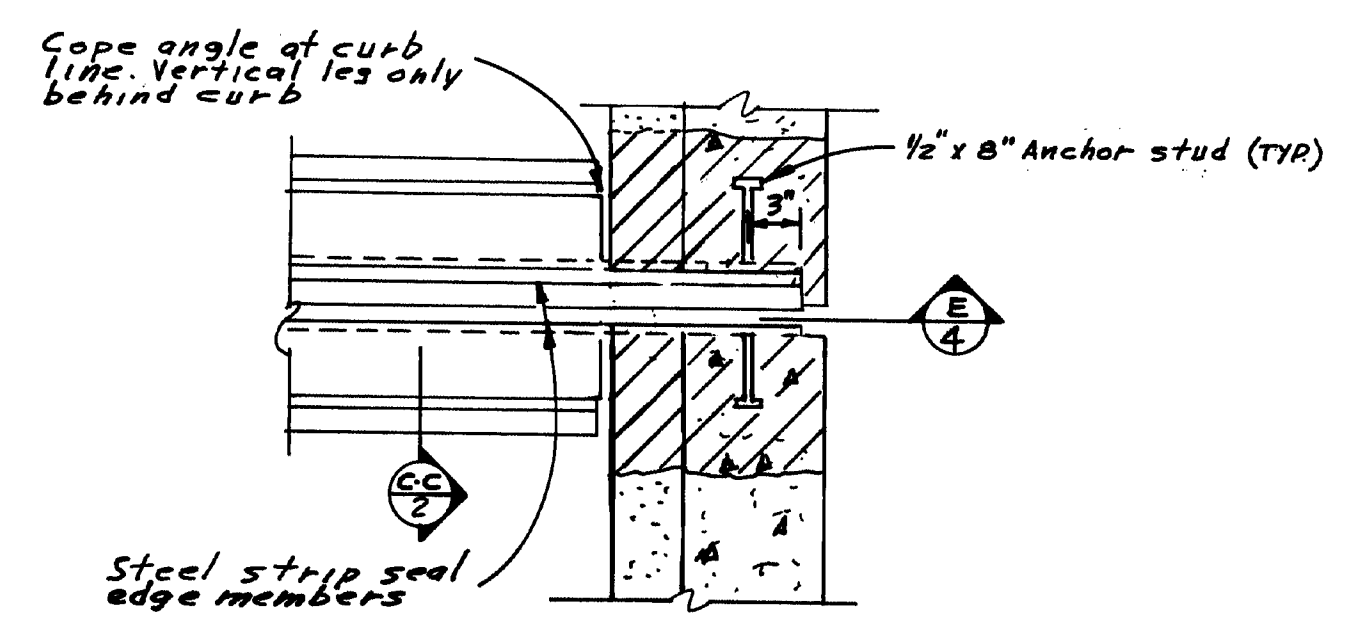
Revised at Ault Dec 1989
Rpm

99-462

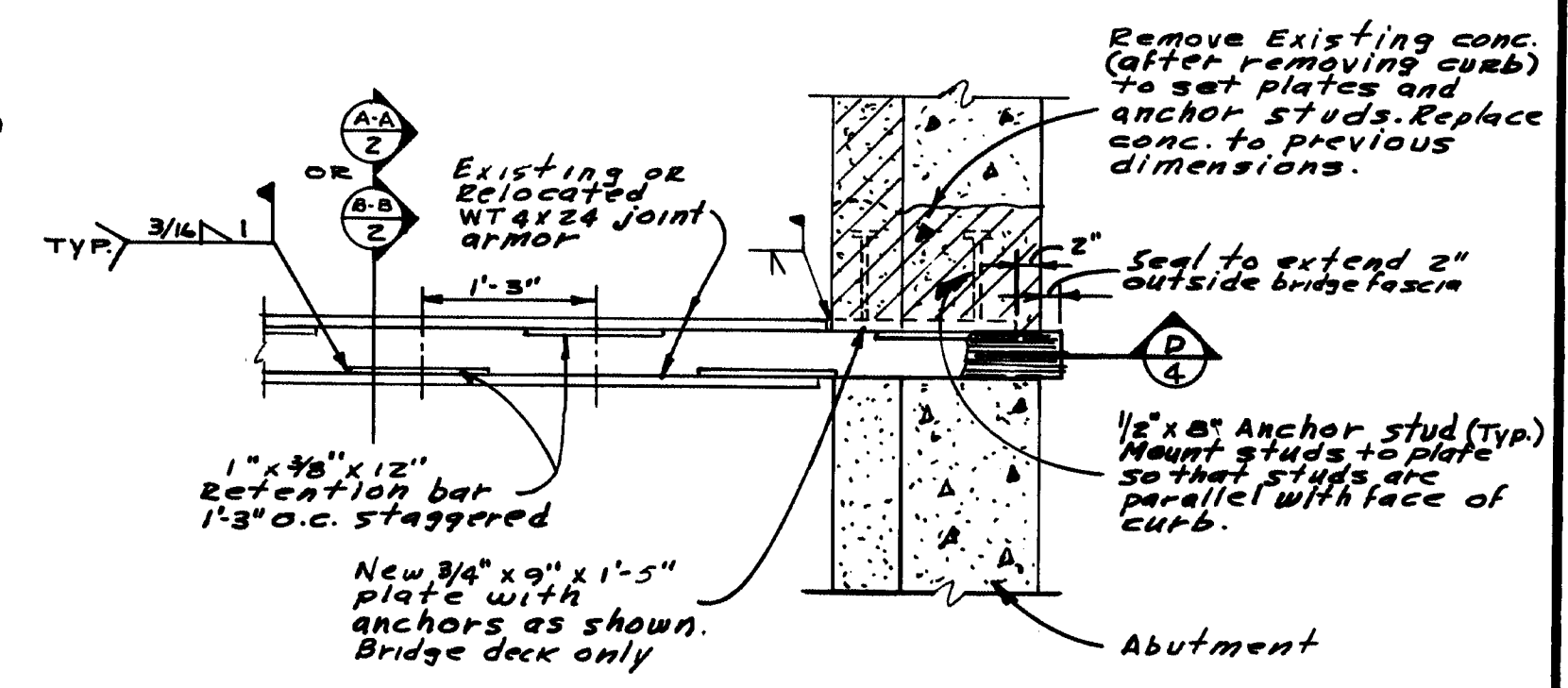
PROJECT DESIGN ENGINEER	BY	DATE
DESIGN-DETAILED	DAK	10-11
CHECKED	BLH	
REVISIONS		
FIELD CHANGES		

BRUNING 44-132 45710-1

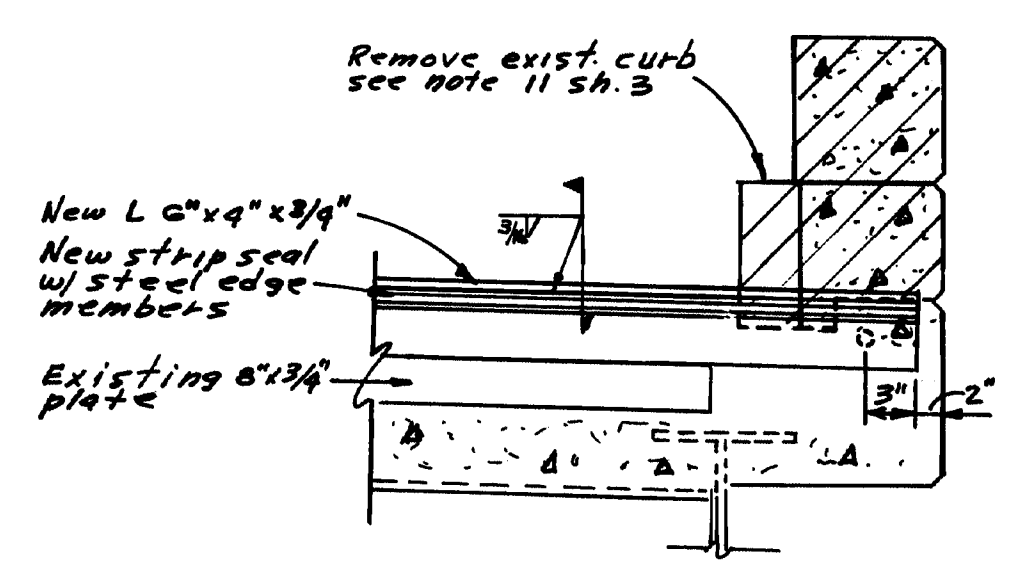
F.R.S. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-7(96)	5	14



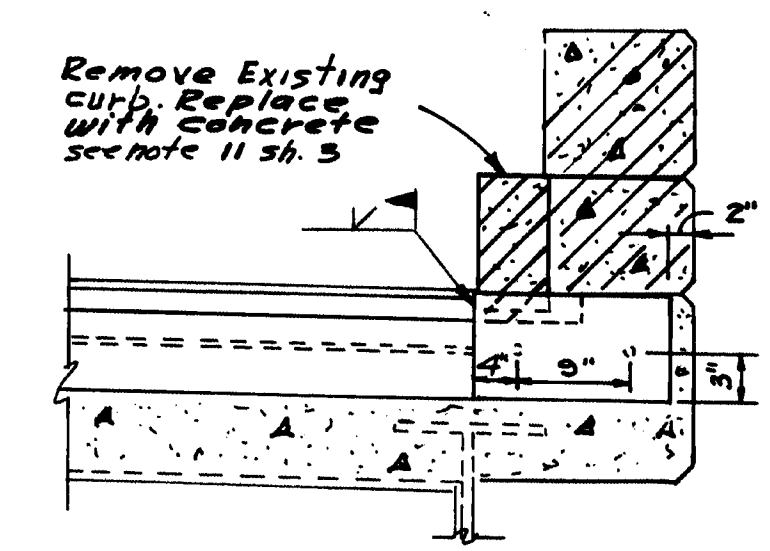
PARTIAL PLAN - GLAND SEAL
See sheet-5 for add'l details



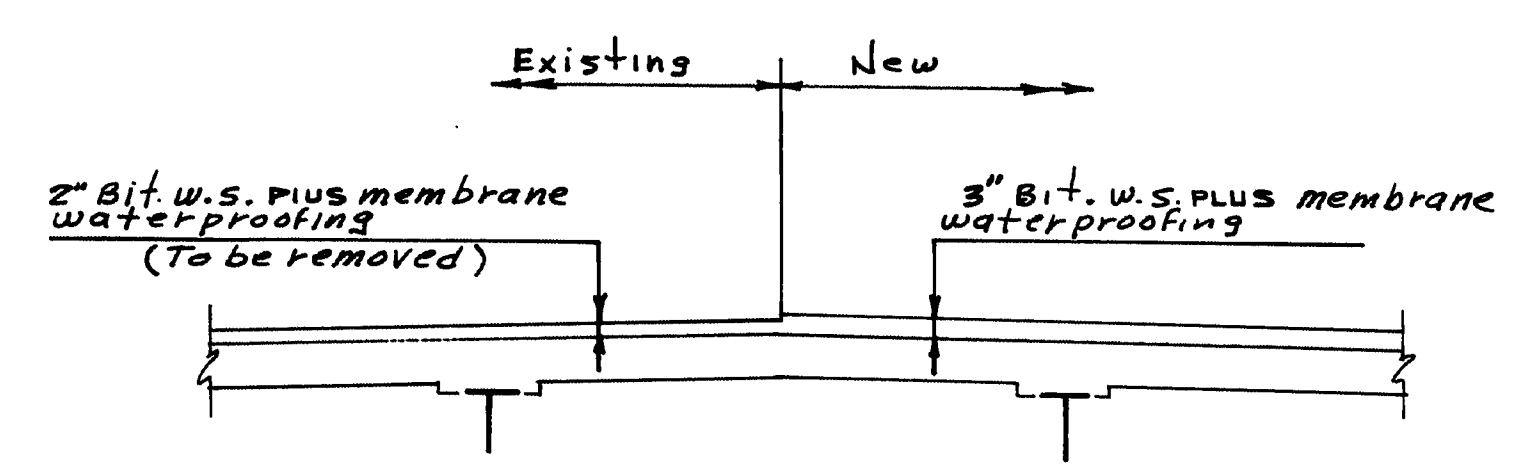
PARTIAL PLAN - COMPRESSION SEAL
See sheet-5 for add'l details



SECTION E



SECTION D



TYPICAL CROSS SECTION

PROJECT DESIGN ENGINEER	DATE
BY	
DESIGN - DETAILED	
CHECKED	
REVISIONS	
FIELD CHANGES	

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

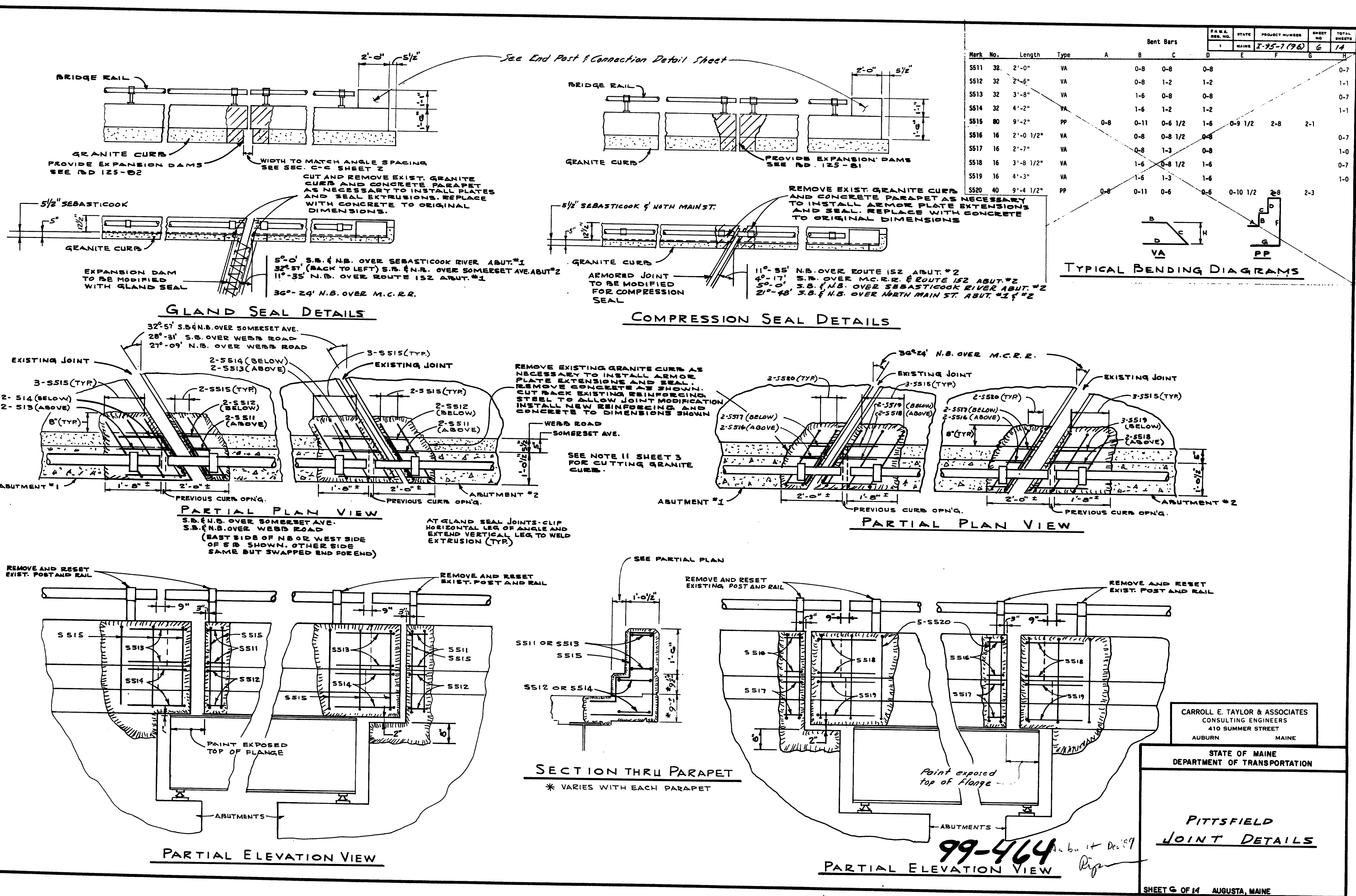
INTERSTATE 95
SOUTHBOUND AND NORTHBOUND
OVER

Sebasticook River, North Main
Street, Webb Road, Somerset
Avenue, Central Maine RR & Route 152
PITTSFIELD

SECTIONS

SHEET 5 OF 14 AUGUSTA, MAINE

99-463
As b. it Dec. 09
Rpn



Mark No.	Length	Type	Bent Bars				A	B	C	D	E	F	G	H
			1	2	3	4								
SS11	32	2'-0"	VA				0-8	0-8	0-8					0-7
SS12	32	2'-6"	VA				0-8	1-2	1-2					1-1
SS13	32	3'-8"	VA				1-6	0-8	0-8					0-7
SS14	32	4'-2"	VA				1-6	1-2	1-2					1-1
SS15	80	9'-2"	PP				0-8	0-11	0-6 1/2	1-6	0-9 1/2	2-8	2-1	
SS16	16	2'-0 1/2"	VA				0-8	0-8 1/2	0-8					0-7
SS17	16	2'-7"	VA				0-8	1-3	0-8					1-0
SS18	16	3'-8 1/2"	VA				1-6	0-8 1/2	1-6					0-7
SS19	16	4'-3"	VA				1-6	1-3	1-6					1-0
SS20	40	9'-4 1/2"	PP				0-8	0-11	0-6	0-6	0-10 1/2	2-8	2-3	

TYPICAL BENDING DIAGRAMS

PROJECT ENGINEER	DATE
DESIGN - DETAILED	BY
CHECKED	PAV
REVISIONS	REV
FIELD CHANGES	CH

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

**PITTSFIELD
JOINT DETAILS**

REINFORCING STEEL SCHEDULE																														
STRAIGHT BARS													BENT BARS																	
MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	O	R	LOCATION				
SEBASTICOOK RIVER NB (END POST) MAINE CENTRAL RR NB (END POST) SOMERSET AVENUE NB (END POST)													TYPICAL AT ALL JOINTS (adjust to fit)																	
H500	32	5'-4"	8 each	H500	16	5'-4"	8 each	H500	32	5'-4"	8 each																			
H501	8	1'-2"	2 each	H501	4	1'-2"	2 each	H501	8	1'-2"	2 each																			
V500	8	1'-5"	2 each	V500	4	1'-5"	2 each	V500	8	1'-5"	2 each																			
V501	1	1'-8"	1	V501	1	1'-8"	1	V501	1	1'-8"	1																			
V502	1	4'-0"	1	V502	1	4'-0"	1	V502	1	4'-0"	1																			
V503	8	4'-4"	2 each (dowel)	V503	4	4'-4"	2 each (dowel)	V503	8	4'-4"	2 each (dowel)																			
V504	16	3'-0"	4 each (dowel)	V504	8	3'-0"	4 each (dowel)	V504	16	3'-0"	4 each (dowel)																			
													5511	32	2'-0"	VA		8"	8"	8"										
													5512	1	2'-6"	1		8"	1'-2"	1'-2"					1'-1"					
													5513	1	3'-8"	1		6"	8"	8"					7"					
													5514	32	4'-2"	VA		6"	1'-2"	1'-2"					1'-1"					
													5515	80	9'-2"	PP	8"	11"	6 1/2"	1'-5"	9 1/2"	2'-8"	2'-1"							
													5516	16	2'-1"	VA		8 1/2"	8 1/2"	8"					7"					
													5517	1	2'-7"	1		8"	1'-3"	8"					1'-0"					
													5518	1	3'-9"	1		1'-6 1/2"	8 1/2"	1'-6"					7"					
													5519	16	4'-3"	VA		1'-6"	1'-3"	1'-6"					1'-0"					
													5520	40	9'-5"	PP	6 1/2"	11"	6"	8"	10 1/2"	2'-8"	2'-3"							

FWA NO. 1	STATE MAINE	PROJECT NUMBER LR-95-7(96)	SHEET NO. 8	TOTAL SHEETS 14
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TYPE-BENDING DIAGRAMS

All dimensions are out to out of reinf. bar.
Bending details and hooks shall conform to the recommendations of the current revision of ACI Standard 318. Δ
Reinforcing Bar: ASTM A615 Grade 60

GENERAL NOTES

- First digit(s) following the letter of the Mark indicates size of reinf. bar.
Mark (A 502) bar size - #5
Mark (P 1001) bar size - #10
Mark (S 603) bar size - #6
- Each truss bar, Type B, may be replaced by two (2) straight bars (one top & one bottom) of the same bar size as the truss bar. Payment in either case shall be based on truss bars as scheduled on plans.

A 111 + Dec 59 *Ryan*

Δ New Bent Bar Type 53	9-26-89
Δ Revised ACI Standard	5-12-83

REVISIONS

REVISIONS	DATE
STATE OF MAINE DEPARTMENT OF TRANSPORTATION	

REINFORCING STEEL SCHEDULE

PITTSFIELD
INTERSTATE 95 OVER
Sebasticoak River North Maine St.
Webb Road, Somerset Avenue,
Central Maine RR & Route 152
Northbound & Southbound

99-466

SHEET 8 OF 14 AUGUSTA, MAINE